

**ADVERTISED COPY**



**30 - 32 WELLINGTON  
ROAD, CLAYTON**

Amended October 2020

**PLANNING REPORT**

## TABLE OF CONTENTS

---

1.	INTRODUCTION .....	1
2.	PROJECT SUMMARY .....	2
	2.1 Planning Scheme Summary.....	2
	2.2 Proposal Summary .....	3
	2.3 Design Statement.....	5
3.	THE SUBJECT SITE AND SURROUNDING DEVELOPMENT .....	6
	3.1 The Subject Site .....	6
	3.2 Immediately Adjoining Properties.....	9
	3.3 Surrounding Development.....	11
	3.4 Locational Attributes and Maps.....	15
4.	PLANNING CONSIDERATIONS .....	17
	4.1 Planning Policy Framework.....	17
	4.2 Residential Growth Zone (Clause 32.07). .....	17
	4.3 Neighbourhood Character (Clause 22.01). .....	18
	4.4 Student Accommodation Use (Clause 22.10). .....	19
	4.5 Traffic, Car Parking, Access and Bicycle Facilities (Clause 22.10, 52.06 & 52.34) .....	20
	4.6 Environmentally Sustainable Development (Clause 22.13 & 53.18). .....	21
	4.7 Waste Management.....	21
5.	APARTMENT DEVELOPMENTS – Clause 58.....	22
6.	CONCLUSION.....	29

## 1. INTRODUCTION

---

Keen Planning has prepared this report on behalf of the applicant, New Clayton Pty Ltd, in support of an application for a five (5) storey Student Accommodation building with basement car parking.

This report should be read in conjunction with the following documentation, which forms the Planning Permit Application submitted to Council:

- Planning Permit Application Form.
- Development Plans, prepared by Telha Clarke Architecture & Design.
- Metropolitan Planning Levy Certificate.
- Certificates of Title.
- Landscape Plan, prepared by Zenith Concepts Landscape Design.
- Traffic Report, prepared by Traffix Group.
- Waste Management Plan, prepared by Leigh Design.
- Arboricultural Report prepared by Dr Peter Yau.
- Survey Plan, prepared by B R Smith Surveyors.
- Sustainable Management Plan, prepared by ADP Consulting.

## 2. PROJECT SUMMARY

### 2.1 Planning Scheme Summary

<b>Address</b>	30 - 32 Wellington Road, Clayton.
<b>Proposal</b>	Construction of a five (5) storey Student Accommodation Building with associated basement car parking.
<b>Zoning</b>	Clause 32.07 – Residential Growth Zone - Schedule 3 (RGZ3).
<b>Overlays</b>	Nil.
<b>Permit Triggers</b>	<p>Clause 32.07 - A permit is required to construct a building for the use as Student Accommodation.</p> <p>Clause 52.06 – A permit is required to reduce the number of car parking spaces.</p> <p>Clause 52.29 – A permit is required to create or alter access to a road in a Road Zone, Category 1.</p>
<b>Applicable Clauses of Planning Scheme</b>	<p><b><u>Planning Policy Framework:</u></b></p> <p>Clause 11 (Settlement).</p> <p>Clause 15 (Built Environment and Heritage).</p> <p>Clause 16 (Housing).</p> <p>Clause 17 (Economic Development).</p> <p>Clause 18 (Transport).</p> <p>Clause 21.01 (Introduction).</p> <p>Clause 21.04 (Residential Development).</p> <p>Clause 21.05 (Economic Development).</p> <p>Clause 21.06 (Major Activity and Neighbourhood Centres).</p> <p>Clause 21.08 (Transport and Traffic).</p> <p>Clause 21.11 (Physical Infrastructure).</p> <p>Clause 22.01 (Residential Development and Character Policy).</p> <p>Clause 22.04 (Stormwater Management Policy).</p> <p>Clause 22.05 (Tree Conservation Policy).</p> <p>Clause 22.10 (Student Accommodation Policy).</p> <p>Clause 22.13 (Environmentally Sustainable Development Policy).</p> <p><b><u>ZONE:</u></b></p> <p>Clause 32.07 (Residential Growth Zone - Schedule 3).</p> <p><b><u>OVERLAYS:</u></b></p> <p>Nil</p> <p><b><u>PARTICULAR / GENERAL PROVISIONS:</u></b></p> <p>Clause 52.06 (Car Parking).</p> <p>Clause 52.29 (Land Adjacent to a Road Zone Category 1).</p> <p>Clause 52.34 (Bicycle Facilities).</p> <p>Clause 53.18 (Stormwater Management in Urban Development).</p> <p>Clause 58 (Apartment Developments).</p> <p>Clause 65 (Decision Guidelines).</p>
<b>Preferred Future Character</b>	Housing Growth Area – Clayton Activity Centre and Monash National Employment Cluster

## 2.2 Proposal Summary

An overview of the proposal is as follows:

- It is proposed to develop the site to be used as a Student Accommodation building which will be marketed towards students studying at Monash University, located directly opposite the site, on the northern side of Wellington Road.
- The development site consists of two allotments that create a development site of 1,457 square metres.
- The building will comprise 102 rooms which will be spread across five stories with one level of basement car parking.
- A maximum building height of 16.2 metres.
- A site coverage of 58 percent.
- The building will be setback from boundaries as follows.
  - Primary (Wellington Road) = 4 metres.
  - Secondary (Irwin Street) = 3.1 – 5.0 metres.
  - Side (west) = 1.8 – 3.6 metres.
  - Rear (south) = 4.5 – 5.8 metres.
- Each room will be a studio room designed for one person and be equipped with a kitchenette, bathroom, study nook, space for a chair and television as well as storage facilities.
- A 50 square metre area of internal communal space is located on the ground floor which opens onto the primary area of outdoor open space located to the east of the building. In addition to the ground level internal communal space, levels 1 - 4 will each have a 25 square metre communal lounge and smaller study booth areas.
- A total of 445 square metres of outdoor communal open space is provided across the site with the primary area located on the eastern side of the building.
- The building has been architecturally designed by Telha Clarke Architecture and will be a positive addition to the Wellington Road area which is an area identified for redevelopment and significant density increase.
- The materials and finishes have taken cues from the immediate neighbourhood character and include a brick finish to the lower levels, which is the dominant material within the locality.
- The siting and massing of the building has carefully considered the adjoining properties and presentation to the street by provided appropriate areas for landscaping to all sides of the site and recessing the fourth and fifth storey.
- Pedestrian access will be provided in the north eastern corner adjacent the intersection of Wellington Road and Irwin Street.
- A vehicular crossover will be provided from Wellington Road via a double width crossover located to the western end of the frontage to the site. This will access a basement car parking area with 25 car parking spaces.
- A total of 52 bicycle spaces are provided across the site (located within the basement and adjacent the entry of the site) available for the use by residents and visitors.
- Existing street trees located on both Wellington Road and Irwin Street will not be impacted by the development.

Perspectives of the proposed development are provided on the following pages.



**Image 1:** *Perspective of the proposed development when viewed from the corner of Wellington Road and Irwin Street (looking south west).*



**Image 2:** *Perspective of the proposed development when viewed from Wellington Road (looking south east).*



**Image 3:** Perspective of the proposed development when viewed from Irwin Street (looking north west).

## 2.3 Design Statement

The following design statement has been prepared by Telha Clark:

*The site is situated on 30-32 Wellington Road in Clayton and its immediate context is residential dwellings. It faces onto a major transport link and the Monash Clayton Campus. In close proximity there are some institutional buildings, short stay accommodation and a major public transport bus terminal.*

*The proposal consists of 102 self contained student accommodation units, a basement carpark and generous common facilities. Particular emphasis has been provided to ensure the scheme delivers a student experience that is in line with the Monash Council guidelines. The large setbacks to the street interfaces provide a green threshold and opportunity for functional communal areas with ample amenity.*

*The material expression of the façade is in direct response to the immediate context of the site. The lower portion of the mass offers a relatable street wall in brick texture which offers a transition to the pedestrian scale and the upper form is dark and recessive to respect the neighbourhood character. Planting and setbacks are carefully utilised to soften the mass and create a form that is appealing.*

*The proposal is very practical with carefully considered access, and sequence for occupants and the unit layouts are functional. The ground floor plan offers an array of social activity and interaction that is carefully curated. The common areas on upper floors have plenty of natural light and ventilation with smaller breakout space/study booths which offer more isolated common areas for the occupants.*

*The scheme aims to create a high standard of student accommodation that is convenient, practical, full of amenity and mindful of the occupants' needs for social interaction.*

### 3. THE SUBJECT SITE AND SURROUNDING DEVELOPMENT

---

#### 3.1 The Subject Site

The subject site consists of two (2) allotments which are located on the south western side of the intersection of Wellington Road (Service Road) and Irwin Street in Clayton. The site fronts onto the Service Road and is separated from the main carriageway by a landscaped median planted with a number of mature trees.

The site is rectangular in shape with a frontage to Wellington Road of 31.9 metres, a frontage to Irwin Street of 45.6 metres and a total area of 1,456 square metres.

The site known as 30 Wellington Road contains a double storey brick dwelling with a pitched tiled roof which is setback 7.6 metres from the Wellington Road frontage. A carport and tandem length garage is constructed to the common boundary with 28 Wellington Road. Vehicle access is via a single width crossover location on the western side of the frontage providing access to the previous mentioned carport and garage. A low brick fence defines the front property boundary.

The site known as 32 Wellington Road is the corner allotment and is dual frontage and contains a single storey brick dwelling with a pitched tiled roof which is setback 7.7 metres from the Wellington Road frontage and 2.2 metres from the Irwin Street frontage. A double garage is located to the rear of the site setback 1.8 metres from the rear (southern) property boundary) and is accessed via a double width crossover opening onto Irwin Street. The Wellington Road frontage is defined by a low brick fence which returns approximately 14 metres along the Irwin Street frontage with the remainder of the Irwin Street frontage defined by a 2 metre high paling fence.



**Image 4:** Aerial photograph showing the subject site and immediately surrounding properties. Source: [www.maps.au.nearmaps.com](http://www.maps.au.nearmaps.com)





**Image 5:** View of 30-32 Wellington Service Road - Looking south.



**Image 6:** View of Subject site from the intersection of Wellington Road and Irwin Street - Looking south-west.



**Image 7:** View from rear of 30 Wellington Road looking south towards 1 Irwin Street.



**Image 8:** View from rear of 30 Wellington Road looking west towards rear of 28 Wellington Road.

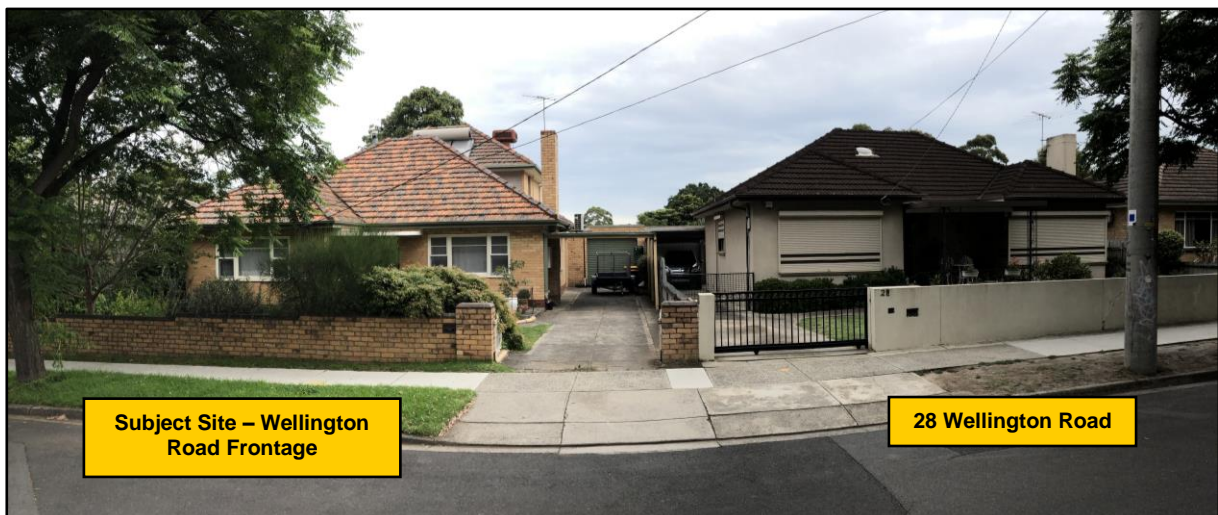


**Image 9:** View from rear of 30 Wellington Road looking south towards 1 Irwin Street.

### 3.2 Immediately Adjoining Properties

The subject site shares a common boundary with two properties known as 28 Wellington Road (side/west) and 1 Irwin Street (rear/south).

The site at 28 Wellington Road (west) contains a single storey concrete rendered dwelling with pitched tiled roof. The dwelling is setback 7.6 metres from the Wellington Road frontage and 2.3 metres from the common boundary (east) with the subject site. A carport and garage is located to the rear of the dwelling and is constructed along approximately 22 metres of the common boundary with the subject site. Vehicle access to the site is via a single width crossover located to the east of the frontage of the site. The front property boundary is defined by a low (1 metre) rendered fence. Landscaping is limited, predominately consisting of bushes within the front setback area and some smaller trees throughout the rear yard.



**Image 10:** Relationship between the subject site and 28 Wellington Road.

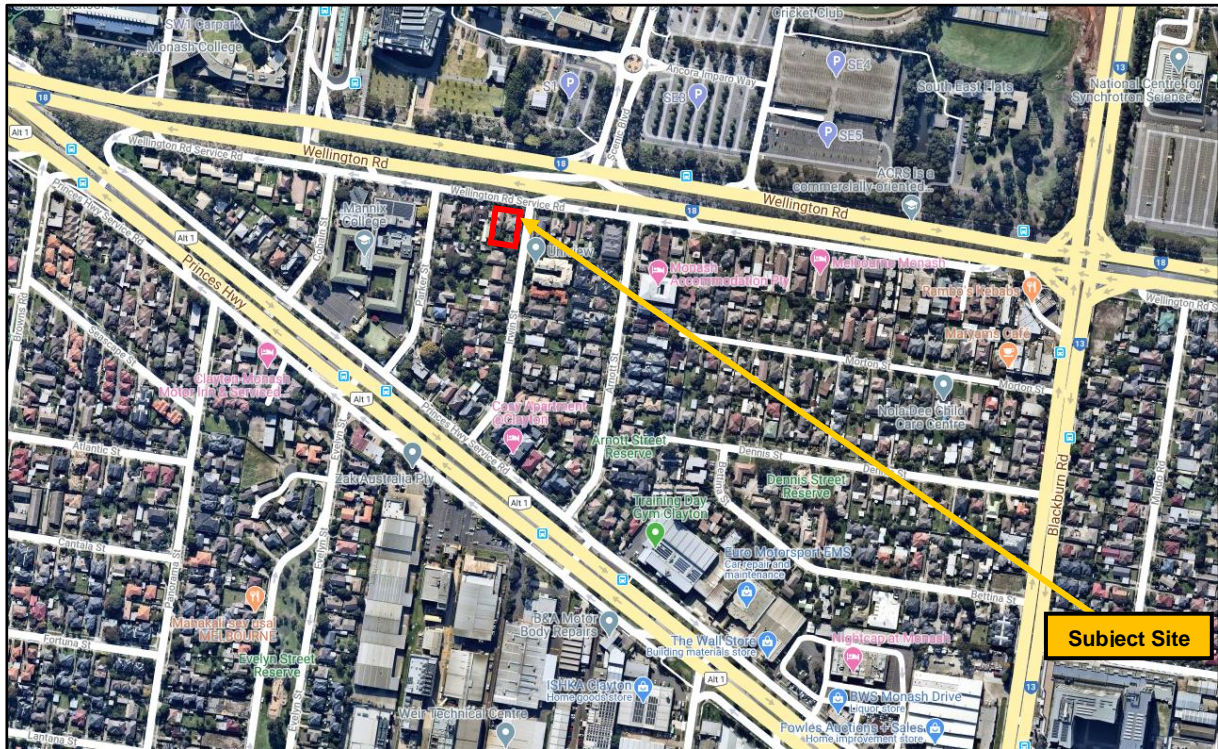
The site at 1 Irwin Street (south) contains a single storey brick dwelling with a pitched tiled roof. The dwelling is setback 8.6 metres from the Irwin Street frontage and 2.7 metres from the common boundary (north) with the subject site. A detached brick garage is located to the rear of the dwelling and constructed along approximately 8 metres of the common boundary with the subject site. Vehicle access to the site is via a single width crossover located to the north of the frontage of the site. The front property boundary is defined by a low (0.7 metre) brick fence. Any vegetation on the site is concentrated to within the front setback area and adjacent to the rear property boundary.



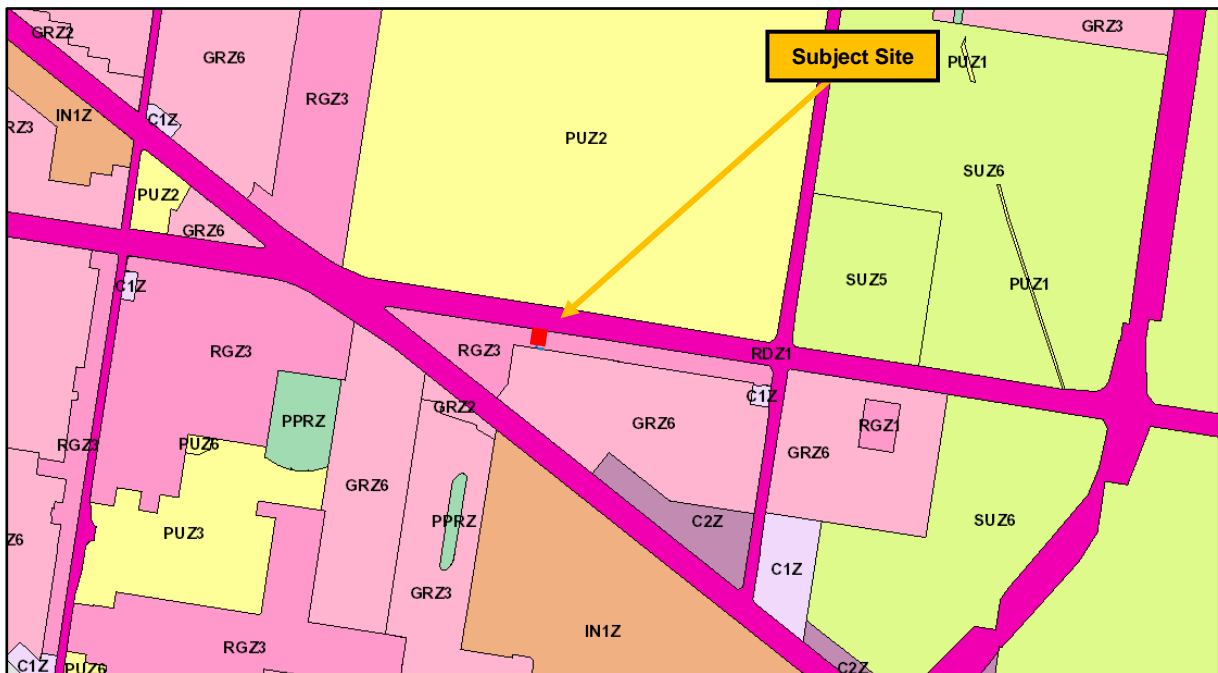
**Figure 11:** Relationship between the subject site and 1 Irwin Street.

### 3.3 Surrounding Development

The surrounding area consists of multiple different zones with each zone including a particular pattern of development and land uses.



**Image 12:** Aerial photograph showing the subject site and surrounding development. Source: [www.maps.au.nearmaps.com](http://www.maps.au.nearmaps.com)



**Image 13:** Zone Map - Source: [mapshare.vic.gov.au](http://mapshare.vic.gov.au)

The land to the north of the site is occupied by the Monash University Campus which covers a significant area and contains multiple-multi storey buildings, car parking areas, bus terminal, playing fields and areas of open space.

To the east and south (across Princes Highway) of the subject site development is predominantly industrial in nature consisting of varying sized warehouse and factory type built form.

Land to the west is predominantly residential consisting of single and double storey dwellings with a reasonable level of infill development also having occurred as a result of the area being well serviced by public transport, shop, services and proximity to the University.

The immediate locality within Wellington Road (north), Princes Highway (south) and Blackburn Road (east) consist of predominately residential land uses with the exception of a strip of commercial properties located towards the intersection of Princes Highway and Blackburn Road. A moderate level of infill development has occurred which has resulted in two storey unit developments or two storey apartment buildings, many of which are used as student accommodation given the proximity of the area to the University.

Mannix College (90 metres west of subject site) is the largest residential development (student accommodation use) within the locality with the site fronting onto both Wellington Road and Princes Highway. The site has been developed with multiple buildings across the site varying in height from 1-3 storeys. The built form incorporates both traditional and more contemporary design elements such as both pitched and flat roof profiles, face brick, white render, metal cladding and vertical battens.

Despite the variety of built form throughout the locality, there are common features including pitched roof profiles, brick or render walls, single vehicle access per site, basement parking (developed sites), consistent front setback and established street trees.



**Image 14:** Apartment development at 34 Wellington Road – opposite site of Irwin Street from subject site.



**Image 15:** Apartment development at 2-4 Irwin Street – diagonally opposite (south east) from subject site.



**Image 16:** Apartment development at 10 Amott Street – 140 metres south east of the subject site.



**Image 17:** Two apartment development at 2 and 6 Arnott Street – 115 metres east of the subject site.



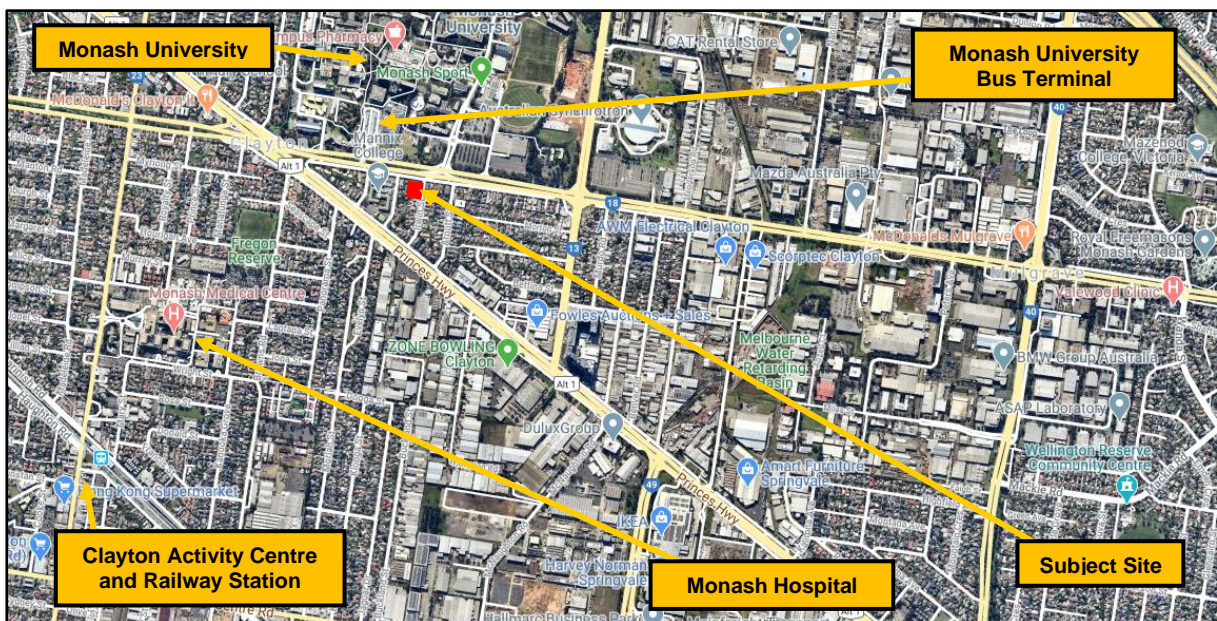
**Image 18:** Mannix College – 90 metres west of subject site – view from intersection of Wellington Road and Packer Street.



### 3.4 Locational Attributes and Maps

The site is in the vicinity of the following facilities:

- The Monash University Clayton Campus is located on the opposite side of Wellington Road.
- The site is located within the Principal Public Transport Network (PPTN) due to its proximity to the Monash University Bus Interchange. This includes bus routes 601, 630, 631, 703, 733, 737, 802, 804, 862, 900 and 978.
- The Monash University Bus Interchange provides a direct connection to the wider Clayton area, Oakleigh, Mulgrave and Springvale where a larger range of shops, services and entertainment facilities are available.
- M City Monash mixed use developed is located 740 metres south east of the subject site which provides access to a variety of shops.
- Monash Medical Centre, including Monash Children's Hospital, is located 800 metres south-west of the subject site.
- Clayton Activity Centre is located approximately 1.4 kilometres south west of the subject site which provides access to a range of services and facilities.
- Clayton Railway Station is located approximately 1.4 kilometres south west of the subject site which provides direct access to the city.
- A number of arterial roads within close proximity of the site provide access to the surrounding suburbs and other major roads, these include; Wellington Road, Princes Highway (approximately 460 metres west of the subject site), Blackburn Road (approximately 540 metres east of subject site), Springvale Road (approximately 2.0 kilometres east of subject site) and the Monash Freeway (4 kilometres east of the subject site).



**Image 19:** Aerial photograph showing the subject site and surrounding attributes. Source: [www.maps.au.nearmaps.com](http://www.maps.au.nearmaps.com)

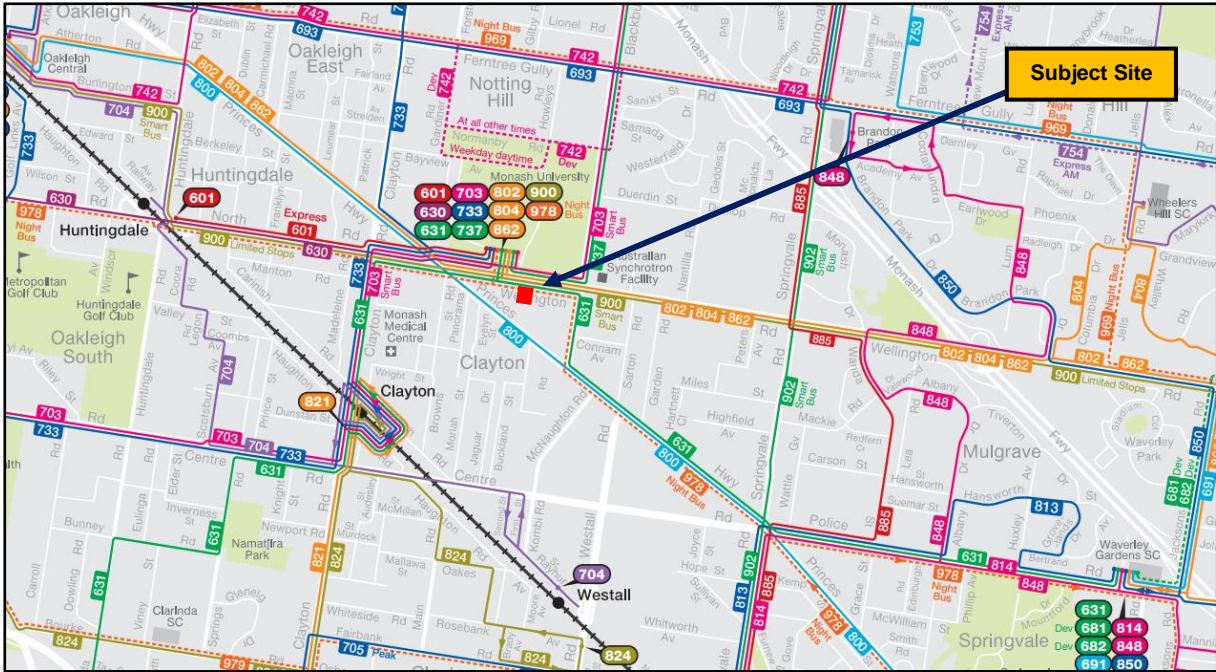


Image 20: Local Public Transport Route Map – Source: [www.ptv.vic.gov.au](http://www.ptv.vic.gov.au)

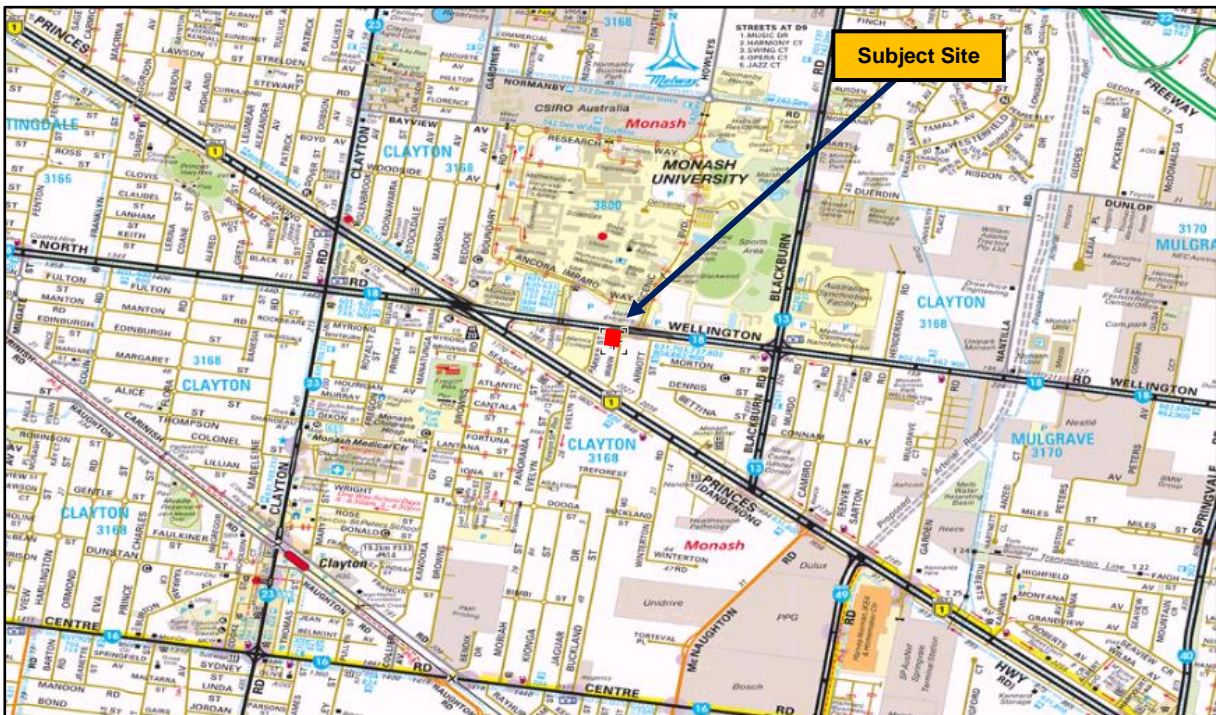


Image 21: Street Directory Map - Source: [www.street-directory.com.au](http://www.street-directory.com.au)

## **4. PLANNING CONSIDERATIONS**

---

The proposal has been assessed under the main considerations of the Monash Planning Scheme. The key areas to be addressed in this section of the report are as follows:

1. Planning Policy Framework.
2. Residential Growth Zone (Clause 32.07).
3. Neighbourhood Character (Clause 22.01).
4. Student Accommodation Use (Clause 22.10).
5. Traffic, Car Parking, Access and Bicycle Facilities (Clause 22.10, 52.06 & 52.34).
6. Environmentally Sustainable Development (Clause 22.13 & 53.18)
7. Waste Management.

### **4.1 Planning Policy Framework**

The proposal is consistent with the applicable provisions of the Planning Policy Framework of the Monash Planning Scheme. In support of this statement, we note the following points:

- The proposal provides Student Accommodation within a preferred location (opposite the Monash University Clayton Campus) which will allow students to be able to walk to University, reducing pressure on existing infrastructure consistent with Clause 15 (Housing) and 22.10 (Student Accommodation) of the Monash Planning Scheme.
- The proposal is located within a highly accessible area, with the major bus interchange located at Monash University facilitating numerous bus routes with the services providing links to the surrounding areas, including Clayton and Huntingdale Train Stations which will encourage public transport use and reduce a reliance on car travel consistent with Clause 21.08 (Transport and Traffic).
- The proposal will accommodate more residents in an established area identified for increased growth, with close proximity to excellent public transport services as well as shops and facilities consistent with Clause 16 (Housing), Clause 17 (Economic Development), Clause 21.04 (Residential Development), 21.05 (Economic Development) and 22.01 (Residential Development and Character Policy).
- The minimum 5.0 metre setback to the low rise residential area to the south combined with the landscape buffer along the common boundary particularly adjacent the rear secluded private open space of 1 Irwin Street will provide a suitable buffer between the properties consistent with Clause 21.04 (Residential Development) and 22.01 (Residential Development and Character Policy).

### **4.2 Residential Growth Zone (Clause 32.07).**

The subject site is located within a Residential Growth Zone Schedule 3 which seeks to achieve increased densities and a larger scale built form which takes advantage of areas which are well serviced while still ensuring that built form respects sensitive residential interfaces and provides sufficient landscaping opportunities.

We submit that the proposed development is consistent with the objectives of zone and in support note the following.

- The development provides a high quality architecturally designed building that will provide 102 student accommodation units in an area that is well serviced by public transport and other services and facilities.
- The building has been designed with appropriate and compliant boundary setbacks which will result in sufficient space around the perimeter of the building capable of being landscaped, including the planting of 22 canopy trees.

- The design of the building has given careful consideration to the adjoining lower scale residential properties to the south which is in a different zone by setting back the first three storeys a minimum of 4.5 metres from the rear boundary with the fourth and fifth storeys further recessed (L4 = 7.3 metres and L5 = 10.3 metres) which reduce the overall scale of the building when viewed from the property to the rear and minimise the extent of any additional overshadowing.
- The recessed upper storeys (level 4 and 5) in combination with the muted colour palette and landscaped roofed areas mean that although the building will have a maximum height of 16.2 metres which exceeds the recommended maximum height of 13.5 metres the additional height will not result in a building that will dominate the streetscape or unreasonably impact any adjoining properties by way of building mass or overshadowing.

### 4.3 Neighbourhood Character (Clause 22.01).

Clause 22.01 (Residential Development and Character Policy) applies to all residential land within the City of Monash and sets out general guidelines for residential development while also separating the municipality into different areas with a more tailored vision of the envisaged future character of a specific location. The subject site is identified as being located within the Housing Growth Area – Clayton Activity Centre and Monash National Employment Cluster preferred future character area which states.

*The scale of new residential development will generally comprise larger footprint apartment developments of a high quality design and finish. Some infill town houses and unit developments will also occur.*

*Where possible on larger sites, developments will be multi level, and set in open gardens. Although setbacks from all boundaries will be less than in common in other parts of Monash, the developments will ensure the incorporation of well maintained landscaping to address the garden city character, albeit in a more urban form.*

Having regard to the above statement, we submit that the proposed development represents an appropriate design response and is consistent with the desired future character of the Housing Growth Area – Clayton Activity Centre and Monash National Employment Cluster and in support note the following.

- The development proposes a well designed apartment type development on a site that is well serviced, capable of and envisaged of supporting a larger scale built form and increased density.
- While the scale and intensity of the proposed development is greater than that of the existing built form character within the locality, it is consistent with the preferred character for the area and is compliant with the setback requirements of the zone. It is however noted that the use of brickwork to the lower podium of the building acknowledges the predominant building material used by existing dwellings within the locality.
- The design of the building has carefully considered the existing low scale built form by providing appropriate and compliant boundary setbacks to the adjoining properties. Furthermore levels four and five of the building have a greater setback and include roof plantings to help soften the building thereby reducing the potential for any adverse amenity impacts on adjoining properties by way of building bulk or overshadowing.
- The development is provided with appropriate and compliant boundary setbacks from all boundaries which are generally in the vicinity of a minimum of 4 - 5 metres. The setback areas provide sufficient space for the planting of 22 canopy trees in addition to other smaller trees, shrubs and ground covers across the site resulting in a development that will positively contribute to the garden city character of the area.

#### **4.4 Student Accommodation Use (Clause 22.10).**

The proposal has been assessed against Clause 22.10 (Student Accommodation) of the Monash Planning Scheme. This Clause of the Planning Scheme provides specific guidelines for the design of student accommodation buildings to ensure that they provide students with a good level of amenity, whilst also reflecting the lifestyles of students.

We submit that the proposed development is consistent with the provisions of Clause 22.10 (Student Accommodation) of the Monash Planning Scheme and note the following in support of this statement:

- Clause 22.10 identifies the site as being a preferred location for Student Accommodation, being within 1500 metres of the tertiary educational institution and within 400 metres of a bus route that access a tertiary educational institution. We note that the subject site is opposite the Monash University and Monash University Bus Terminal therefore being an ideal location for the proposed use.
- The built form represents a high level of architectural merit and includes generous building setbacks to the street and existing low scale residential properties, emphasised lower podium with recessed upper stories and areas of landscaping that will result in a development that will positively contribute to the Wellington Road and Irwin Street streetscape and is consistent with the preferred neighbourhood character for the area. In addition, the development will also include elements such as the use of brick (predominant material within the locality) that respect the existing character of the area.
- The design incorporates internal and external communal areas for recreation to foster interaction between students, whilst personal and communal study areas will allow students to undertake tertiary activities in quiet locations.
- All rooms will include windows that open to an external area, as well as kitchenettes, bathrooms, study spaces and room for beds. All rooms will exceed the specified minimum area of 24 square metres (including six accessible rooms 40 square metres in size), include internal and power point connections and will be naturally ventilated, providing a diverse range of options for students. This will ensure that each room is provided with good amenity.
- The development will be provided with 445 square metres of external communal open space located to the side and rear of the building which exceeds the 420 square metres required by the Clause. The primary area of outdoor space will be located on the eastern side of the building and be directly accessible from the main internal common area. The size and location of outdoor spaces throughout the development are considered appropriate for Student Accommodation and will meet the recreational needs of future residents.
- The interface to Wellington Road and Irwin Street includes ample landscaping, generous levels of glazing to encourage interaction and passive surveillance of the street and a readily viewable entrance. This will increase safety around the environs of the building, particularly when entering and exiting the building during early mornings or late evenings.
- A range of room sizes will be provided, including accessible rooms.
- An appropriate level of car parking is proposed at a rate of 0.25 spaces per bed, which takes into account that students will not be required to drive given the close proximity to Monash University and the availability of nearby public transport options.
- Bicycle storage facilities in excess of the requirement will be provided within the basement for residents with additional visitor bicycle parking spaces provided adjacent to the building entry.

For the above reasons, we submit the location and design of the student accommodation building is appropriate and consistent with the Clause.

#### **4.5 Traffic, Car Parking, Access and Bicycle Facilities (Clause 22.10, 52.06 & 52.34)**

*This section of the Report should be read in conjunction with the Traffic Report prepared by Traffix Group.*

The proposal has been assessed against the car parking/bicycle facilities requirements within Clause 22.10 (Student Accommodation), 52.06 (Car Parking) and Clause 52.34 (Bicycle Facilities) of the Monash Planning Scheme. Whilst no car parking rate is specified for Student Accommodation within Clause 52.06 (Car Parking), Clause 22.10 (Student Accommodation) specifies that 0.3 spaces are to be provided for sites located within preferred locations (such as the subject site).

Based on the 102 beds provided, this generates a requirement for 30 spaces however 25 space are proposed to be provided which equates to a rate of 0.25 spaces per bed.

A Traffic Assessment has been provided by Traffix Group which concludes that the dispensation of five (5) space can be supported in this instance for the following reasons.

- There is a high demand for student housing with minimal provision of car parking, at rates lower than required by the Monash local policy.
- The site is located in close proximity to public transport services including multiple bus services that pass through the University as do all services travelling along Wellington Road and Princes Highway.
- The site is a short bus trip or a 20 minute walk from the Clayton Railway Station. It is also noted that both Clayton and Monash University are proposed to be included within the suburban rail loop with work due to commence in 2022.
- The development includes a high provision of bicycle parking with more than one space for every two students.
- Any on street parking impacts will be negligible as on street parking in the vicinity of the site is highly restricted to short term parking. We also note that students would not be eligible for resident parking permits.

In addition to the above, the Traffic assessment and report also concludes that the access arrangements and traffic generation, the design and layout of the basement and bicycle storage facility proposed are consistent with the objectives and guidelines of Clause 22.10 (Student Accommodation), 52.06 (Car Parking) and Clause 52.36 (Bicycle Facilities). We provide the following summary.

- A total of 52 on-site bicycle parking spaces are provided which exceeds the statutory requirements under Clause 22.10 and Clause 52.34. The bicycle parking layout also complies with AS2890.3-2015.
- The level of traffic generated as a result of the development will be low, spread throughout the day, residential in nature and have a negligible impact on the operation or safety of Irwin Street, Wellington Service Road or the surrounding road network.

For the above reasons, the access, parking provision and car parking facilities are considered to be appropriate and consistent with the guidelines of the relevant Clauses.

#### **4.6 Environmentally Sustainable Development (Clause 22.13 & 53.18).**

*This section of the Report should be read in conjunction with the Sustainable Design Assessment prepared by ADP Consulting.*

The development is consistent with the Objectives set out in Clause 22.13 (Environmentally Sustainable Development) and Clause 53.18 (Stormwater Management in Urban Developments). A Sustainable Management Plan has been prepared and included as supporting documentation to this planning permit application outlining the environmentally sustainable design initiatives which result in the development achieving a Green Star Rating of 66 and a STORM Rating of 103 percent.

#### **4.7 Waste Management**

*This section of the Report should be read in conjunction with the Waste Management Plan prepared by Leigh Design.*

A Waste Management Plan (WMP) has been prepared by Leigh Design and accompanies this application. The report details the proposed storage and collection of waste on site and a summary of this report is as follows:

- Students will dispose of waste into the waste receptacles provided on each floor. The building operator will then transfer these to the bins within the basement.
- Each student is anticipated to generate a demand for 6.12 cubic metres of garbage per week. Three garbage and three recyclable bins with a 1,100 litre capacity in addition to five organic bins with a capacity of 240 litre capacity will appropriately cater to this demand.
- Waste will be collected twice weekly.
- Waste will be collected by a private contractor from the bin storage room within the basement car park area. The contractor will transfer waste between the waste storage area and the vehicle.
- A rear lift vehicle which is 2.1 metres in height and 6.4 metres long will collect the waste and swept path diagrams show that the vehicle can manoeuvre within the basement.

For the above reasons, the proposed storage and collection of waste is considered appropriate.

## 5. APARTMENT DEVELOPMENTS – Clause 58

58.01 – URBAN CONTEXT REPORT AND DESIGN RESPONSE	
58.01-2 Urban Context	<p><b>Complies</b></p> <p>Refer to Section 3 of this report and the Development Plans prepared by Telha Clarke Architecture.</p>
58.01-3 Design response	<p><b>Complies</b></p> <p>Refer to Section 3 and Section 4 of this report and the Development Plans prepared by Telha Clarke Architecture.</p>
58.02 – URBAN CONTEXT	
<p><b>58.02-1 - Standard D1</b></p> <p>Urban Context objectives</p> <ul style="list-style-type: none"> <li>• <i>To ensure that the design responds to the existing urban context or contributes to the preferred future development of the area.</i></li> <li>• <i>To ensure that the development responds to the features of the site and the surrounding area.</i></li> </ul>	<p><b>Complies</b></p> <p>Refer to Section 3 and 4 of this report and the development plans prepared by Telha Clarke Architecture.</p>
<p><b>58.02-2 -Standard D2</b></p> <p>Residential policy objectives</p> <ul style="list-style-type: none"> <li>• <i>To ensure that residential development is provided in accordance with any policy for housing in the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.</i></li> <li>• <i>To support higher densities in areas where development can take advantage of public and community infrastructure and services.</i></li> </ul>	<p><b>Complies</b></p> <p>Refer to Section 3 and 4 of this report and the development plans prepared by Telha Clarke architecture.</p>
<p><b>58.02-3 - Standard D3</b></p> <p>Dwelling diversity objective</p> <ul style="list-style-type: none"> <li>• <i>To encourage a range of dwelling sizes and types in developments of ten or more dwellings.</i></li> </ul>	<p><b>Complies</b></p> <p>The proposal includes varying sized (24 - 40 square metres) student rooms which all have one (1) bedroom with all rooms contain a kitchenette, shower, toilet and washbasin and study area.</p> <p>Several student rooms also include DDA compliant bathrooms to allow access for mobility impaired persons.</p>
<p><b>58.02-4 - Standard D4</b></p> <p>Infrastructure objectives</p> <ul style="list-style-type: none"> <li>• <i>To ensure development is provided with appropriate utility services and infrastructure.</i></li> <li>• <i>To ensure development does not unreasonable overload the capacity of utility services and infrastructure.</i></li> </ul>	<p><b>Complies</b></p> <p>The development can be connected to and there is no evidence to suggest it will overload existing services.</p>



<p><b>58.02-5 - Standard D5</b> Integration with the street objective</p> <ul style="list-style-type: none"> <li><i>To integrate the layout of development with the street.</i></li> </ul>	<p><b>Complies</b></p> <p>It is considered that the proposed development will integrate into the Wellington Road and Irwin Street streetscapes for the following reasons.</p> <ul style="list-style-type: none"> <li>The entry to the building is located in the north eastern corner of the site providing direct access to both Wellington Road and Irwin Street via a clearly defined paved area.</li> <li>Any boundary fencing will be an open metal picket style with landscaping behind and limited to a height of 1.2 metres along Wellington Road and 1.5 metres along Irwin Street which will assist with maintaining an appropriate connection between the development and the street.</li> <li>Numerous rooms have windows overlooking both Wellington Road and Irwin Street with a number of rooms also having small balconies facing the street.</li> <li>Generous landscaping areas are proposed along both street setbacks and include the provision of large canopy trees and ground planting.</li> </ul>
<p><b>58.03 - SITE LAYOUT</b></p>	
<p><b>58.03-1 - Standard D6</b> Energy efficiency objectives</p> <ul style="list-style-type: none"> <li><i>To achieve and protect energy efficient dwellings and residential buildings.</i></li> <li><i>To ensure the orientation and layout of development reduce fossil fuel energy use and make appropriate use of daylight and solar energy.</i></li> <li><i>To ensure dwellings achieve adequate thermal efficiency</i></li> </ul>	<p><b>Complies</b></p> <p>Refer to the Sustainable Management Plan prepared by ADP Consulting Engineers and included as an attachment to this report.</p>
<p><b>58.03-2 - Standard D7</b> Communal open space objective</p> <p><i>To ensure that communal open space is accessible, practical, attractive, easily maintained and integrated with the layout of the development.</i></p>	<p><b>Complies</b></p> <p>The communal open space associated with the development will be easily accessible for all residents as it is located at ground level and can be accessed via multiple points throughout the development.</p>
<p><b>58.03-3 - Standard D8</b> Solar access to communal outdoor open space objective</p> <ul style="list-style-type: none"> <li><i>To allow solar access into communal outdoor open space.</i></li> </ul>	<p><b>Complies</b></p> <p>The proposal incorporates communal open space located at ground level located to the south, east and west of the building. Providing multiple areas located on different orientations of the site maximises the available solar access throughout the day to these areas.</p>
<p><b>58.03-4 - Standard D9</b> Safety objective</p> <p><i>To ensure the layout of development provides for the safety and security of residents and property.</i></p>	<p><b>Complies</b></p> <p>The pedestrian entrance to the building is clearly visible from both Wellington Road and Irwin Street. The entry area includes a porch for protection and will include adequate lighting for safety at night. The entry area also has several habitable room windows and balconies that overlook this area that provides passive surveillance.</p> <p>In addition, perimeter fencing will be provided to both street frontages (gate access restricted to maintenance use), which enhances the security of the site and well being for the residents.</p>

<p><b>58.03-5 - Standard D10</b> Landscaping objectives</p> <ul style="list-style-type: none"> <li>• <i>To encourage development that respects the landscape character of the neighbourhood.</i></li> <li>• <i>To encourage development that maintains and enhances habitat for plants and animals in locations of habitat importance.</i></li> <li>• <i>To provide appropriate landscaping.</i></li> <li>• <i>To encourage the retention of mature vegetation on the site.</i></li> <li>• <i>To promote climate responsive landscape design and water management in developments that support thermal comfort and reduces the urban heat island effect.</i></li> </ul>	<p><b>Variation Sought</b></p> <p>A Landscape Plan prepared by Zenith Concepts Landscape Designs accompanies this application and demonstrates the proposed landscape concept for the site.</p> <p>Although the development does not propose a canopy tree with a mature height similar to the height of the building we submit that overall the landscape design will positively contribute to the existing garden character of the area and in support we note the following.</p> <ul style="list-style-type: none"> <li>• There are only two notable existing trees on the site with one having a medium retention value. The tree is centrally located on the site and would restrict any reasonable development of the site.</li> <li>• Six street trees surrounding the development site will not be impacted by the development and will assist with the preservation of a treed setting for the development.</li> <li>• 22 canopy trees are proposed to be planted across the site to compensate for the removal of any existing vegetation.</li> <li>• A variety of shrubs, ground covers and strappy plants will complement the canopy trees proposed for the site.</li> <li>• The proposed vegetation incorporates a mix of native and exotic species which will complement to existing landscape character of the area.</li> <li>• Areas of landscaping are proposed to each of the street frontages of the site which will help to soften the appearance of the development.</li> <li>• In addition to ground level landscaping, plantings are proposed on sections of the third (9 metres above ground level) and fourth storey (12 metres above ground level) roof areas which will add additional greenery to the development.</li> </ul>
<p><b>58.03-6 - Standard D11</b> Access objectives</p> <ul style="list-style-type: none"> <li>• <i>To ensure the number and design of vehicle crossovers respects the urban context.</i></li> </ul>	<p><b>Complies</b></p> <p>Refer to Section 4.5 of this report.</p>
<p><b>58.03-7 - Standard D12</b> Parking location objectives</p> <ul style="list-style-type: none"> <li>• <i>To provide convenient parking for resident and visitor vehicles.</i></li> <li>• <i>To protect residents from vehicular noise within developments.</i></li> </ul>	<p><b>Complies</b></p> <p>Refer to Section 4.5 of this report.</p>

<p><b>58.03-8 - Standard D13</b> Integrated water and stormwater management objectives</p> <ul style="list-style-type: none"> <li>To encourage the use of alternative water sources such as rainwater, stormwater and recycled water.</li> <li>To facilitate stormwater collection, utilisation and infiltration within the development.</li> <li>To encourage development that reduces the impact of stormwater run-off on the drainage system and filters sediment and waste from stormwater prior to discharge from the site.</li> </ul>	<p><b>Complies</b> Refer to Section 4.6 of this report and the Sustainable Design Assessment prepared by ADP Consulting.</p>
<p><b>58.04 - AMENITY IMPACTS</b></p>	
<p><b>58.04-1 - Standard D14</b> Building Setback objective</p> <ul style="list-style-type: none"> <li><i>To ensure the setback of the building from a boundary appropriately responds to the existing urban context or contributes to the preferred future development of the area.</i></li> <li><i>To allow adequate daylight into new dwellings.</i></li> <li><i>To limit views into habitable room windows and private open space of new and existing dwellings.</i></li> <li><i>To provide a reasonable outlook from new buildings.</i></li> <li><i>To ensure the building setbacks provide appropriate internal amenity to meet the needs of residents.</i></li> </ul>	<p><b>Complies</b> The proposed building setbacks are appropriate in the urban context and are consistent with the objectives and standard. In support we note the following.</p> <ul style="list-style-type: none"> <li>The proposed building setbacks will result in a built form that is consistent with the preferred future character of the area.</li> <li>The building will have a primary setback of 4 metres to Wellington Road and secondary setback of 3 to 5 metres to Irwin Street which complies with the recommended setbacks within the schedule to the zone.</li> <li>The side and rear setbacks comply with the setbacks specified within the schedule to the zone. Refer to setback diagrams on the development plan elevations.</li> <li>The recessed fourth and fifth storey from the side and rear boundaries will assist with minimising any perceived amenity impacts on adjoining lower scale residential properties.</li> <li>The provided setbacks allow opportunities on each side of the building for the planting of canopy trees that will grow to a height of 7 metres and/or larger screening shrubs and roof plantings which will assist with softening the presentation of the built form.</li> <li>Appropriate boundary setbacks are provided to ensure that each room within the development can receive adequate solar access.</li> </ul> <p>For these reasons, we submit that the proposed setbacks are appropriate and consistent with the preferred future neighbourhood character of the area.</p>

<p><b>58.04-2 - Standard D15</b> Internal views objective</p> <ul style="list-style-type: none"> <li>To limit views into the private open space and habitable room windows of dwellings within a development.</li> </ul>	<p><b>Complies</b></p> <p>The development has been designed to limit the potential for overlooking into habitable room windows within the development. Where required landscaping buffers are provided at ground level to restrict views into rooms from communal areas and vertical angled louvres are provided to the units overlooking the central courtyard to restrict views between rooms.</p>
<p><b>58.04-3 - Standard D16</b> Noise impacts objectives</p> <ul style="list-style-type: none"> <li>To contain noise sources in developments that may affect existing dwellings.</li> <li>To protect residents from external and internal noise sources</li> </ul>	<p><b>Complies</b></p> <p>Noise sources, such as air conditioners, will be located on the rooftop and within the basement to ensure they do not unreasonably impact the amenity of existing or proposed bedrooms on the subject site or adjoining sites.</p> <p>Furthermore, the new air-conditioning units will comply with relevant noise requirements for residential developments.</p> <p>We note that all windows will be double glazed to ensure future residents will not be affected by external noise source such as traffic from Wellington Road. Is it therefore considered that there will be no unreasonable noise impacts to the future residents of this proposal.</p>
<p><b>58.05-1 - Standard D17</b> Accessibility objective</p> <ul style="list-style-type: none"> <li>To ensure the design of dwellings meets the needs of people with limited mobility</li> </ul>	<p><b>Complies</b></p> <p>The building is provided with a lift to ensure the entire building is readily accessible for people with limited mobility with six accessible rooms are also provided.</p>
<p><b>58.05-2 - Standard D18</b> Building entry and circulation objectives</p> <ul style="list-style-type: none"> <li>To provide each dwelling and building with its own sense of identity.</li> <li>To ensure the internal layout of buildings provide for the safe, functional and efficient movement of residents.</li> <li>To ensure internal communal areas provide adequate access to daylight and natural ventilation.</li> </ul>	<p><b>Complies</b></p> <p>It is considered that the proposed development meets this standard for the following reasons:</p> <ul style="list-style-type: none"> <li>The development is orientated towards both street frontages and is provided with a clearly identifiable pathway and entrance leading to a common lobby.</li> <li>An entry portico is provided at the entrance to the building which provides shelter.</li> <li>The internal communal entry incorporates a significant level of glazing to take advantage of natural light and also include multiple openings to assist with natural ventilation.</li> </ul>

<p><b>58.05-3 - Standard D19</b> Private open space objective</p> <ul style="list-style-type: none"> <li>To provide adequate private open space for the reasonable recreation and service needs of residents.</li> </ul>	<p><b>Variation Sought</b></p> <p>Although the proposal does not meet the quantitative requirements of the standard with regards to private open space it does meet the open space requirements within the Student Accommodation Policy (Clause 22.10) which is more applicable to the proposed development.</p> <p>The level of open space provided throughout the site exceeds that required by Clause 22.10 and will meet the recreational needs of the future resident. In addition we note the following.</p> <ul style="list-style-type: none"> <li>A 445 square metre area of communal open space will be provided (420 square metres required) at ground level which can be conveniently accessed from within the building.</li> <li>Multiple areas of communal outdoor space are provided on the east, west and south of the building thereby providing a variety of usable areas and maximising solar access throughout the day.</li> <li>In addition to the communal open space, 23 of the rooms will also be provided with a private four square metre balcony. We note that usable balconies will not be provided above the third level as recommended by Clause 22.10 with the Juliet balconies to the fourth and fifth storeys used only as a design feature increase façade articulation.</li> <li>Significant areas of open space are located on Monash University campus grounds directly opposite the site, including playing fields and passive recreational areas.</li> </ul> <p>For these reasons, we submit that the provision of open space across the site is reasonable and will meet the recreational needs of residents.</p>
<p><b>58.05-4 - Standard D20</b> Storage objective</p> <ul style="list-style-type: none"> <li>To provide adequate storage facilities for each dwelling.</li> </ul>	<p><b>Complies</b></p> <p>Each of the rooms will be provided with 8 cubic metres of storage which complies with the requirements for studio apartments.</p>
<p><b>58.06 – Detailed Design</b></p>	
<p><b>58.06-1 - Standard D21</b> Common Property Objectives</p> <ul style="list-style-type: none"> <li>To ensure that communal open space, car parking, access areas and site facilities are practical, attractive and easily maintained.</li> <li>To avoid future management difficulties in areas of common ownership.</li> </ul>	<p><b>Complies</b></p> <p>Areas of common property such as the communal open space, basement car park, entrance path and internal corridors/lobby can be managed efficiently in accordance with the requirements of this standard.</p>
<p><b>58.06-2 - Standard D22</b> Site services objectives</p> <ul style="list-style-type: none"> <li>To ensure that site services can be installed and easily maintained.</li> <li>To ensure that site facilities are accessible, adequate and attractive.</li> </ul>	<p><b>Complies</b></p> <p>Mail boxes will be located adjacent to the pedestrian entrance within the reception area.</p> <p>Provision for bin facilities are located adjacent to the storage facilities within the basement.</p> <p>Building services and meters are conveniently located within the site and where possible any services that are required to front onto the street have been separated and screened to minimise their dominance within the streetscape.</p>

<p><b>58.06-3- Standard D23</b> Waste and recycling objectives</p> <ul style="list-style-type: none"> <li>• <i>To ensure dwellings are designed to encourage waste recycling.</i></li> <li>• <i>To ensure that waste and recycling facilities are accessible, adequate and attractive.</i></li> <li>• <i>To ensure that waste and recycling facilities are designed and managed to minimise impacts on residential amenity, health and the public realm.</i></li> </ul>	<p><b>Complies</b> Refer to section 4.7 of this report and the Waste Management Plan prepared by Leigh Design and included as an attachment to this report.</p>
<p><b>58.07 – INTERNAL AMENITY</b></p>	
<p><b>58.07-1 - Standard D24</b> Functional layout objective</p> <ul style="list-style-type: none"> <li>• <i>To ensure dwellings provide functional area that meet the needs of residents.</i></li> </ul>	<p><b>Complies</b> <u>Bedroom Dimensions</u> This is not applicable given the student rooms proposed are studio apartments, which incorporate beds within living areas. Nonetheless, each room will comply with the specifications within Clause 22.10 (Student Accommodation Policy) by having a minimum area of 24 square metres to allow for the provision of a larger sized bed and suitable circulation space within the room. We also note that a number of larger rooms in the vicinity of 40 square metres will also be provided.</p>
<p><b>58.07-2 - Standard D25</b> Room depth objective</p> <ul style="list-style-type: none"> <li>• <i>To allow adequate daylight into single aspect habitable rooms.</i></li> </ul>	<p><b>Complies</b> All single aspect bedrooms at a minimum of 24 square metres comply with the standard. A daylight assessment has also been carried out by ADP Consulting, which demonstrates that each unit will receive adequate daylight. Refer to SDA report for further details.</p>
<p><b>58.07-3 - Standard D26</b> Windows objective</p> <ul style="list-style-type: none"> <li>• <i>To allow adequate daylight into new habitable room windows.</i></li> </ul>	<p><b>Complies</b> All habitable rooms include a window in an external wall of the building consistent with the requirements of this standard.</p>
<p><b>55.07-4 - Standard D27</b> Natural ventilation objectives</p> <ul style="list-style-type: none"> <li>• <i>To encourage natural ventilation of dwellings.</i></li> <li>• <i>To allow occupants to effectively manage natural ventilation of dwellings.</i></li> </ul>	<p><b>Complies</b> All rooms will have an external opening to provide ventilation and allow occupants to manage the ventilation within the room.</p>

## 6. CONCLUSION

---

It is submitted that the proposed development is appropriate for the subject site and is consistent with the future planning of the area. In support of this statement, the following points are noted.

- The proposed development is consistent with the strategic directions of the Monash Planning Scheme and State and Local Planning Policy Frameworks, by providing a development that will appropriately increase density in an area that is well served by facilities and public transport.
- The proposed site is identified by Clause 22.10 (Student Accommodation Policy) as being a preferred location for student accommodation given the close proximity to the University and regular public transport services.
- The proposed development is consistent with the envisaged Neighbourhood Character for the area. Clause 22.01 (Residential Development and Character Policy) identifies the site as being within the Housing Growth Area – Clayton Activity Centre and Monash National Employment Cluster which seeks larger scale multi-level developments as proposed.
- The proposed building represents a high level of architectural merit with built form elements including an emphasised lower level podium finished in brickwork, recessed upper storeys finished in a darker colour to differentiate it from the section below, varied boundary setbacks and substantial areas of glazing. The combination of these elements will result in a building that will positively contribute to the existing streetscape and is consistent with the envisaged character for the area.
- The potential for any adverse amenity impacts to adjoining properties by way of building bulk and overshadowing has been minimised through the use of appropriate/compliant building setbacks, significantly recessed upper storeys and landscape buffers.
- Although the scale and intensity of the development is greater than the existing built form within the area, the development still respects the existing character of the area through the use of brickwork (predominant building material) and substantial landscaping across the site.
- This proposal meets the relevant objectives and is highly compliant with the standards of ResCode and provides a site responsive design that is sensitive to the amenity of adjoining properties and the existing and preferred character of the area.
- The development has been provided with multiple areas of open space which are of sufficient size and orientation to meet the passive and active recreational needs of future residents.
- The development will be provided with an appropriate level of car parking to meet the expected demand generated considering its locational context close to the University and other services and facilities meaning that residents will not be reliant on cars as a primary mode of transport.



**KEEN PLANNING**  
October 2020