

7.1.4 MONASH BOULEVARDS UDF IMPLEMENTATION - AMENDMENT C172 - CONSIDERATION OF SUBMISSIONS

Responsible Manager:	Sherry Hopkins, Acting Manager Strategic Planning
Responsible Director:	Peter Panagakos, Director City Development

RECOMMENDATION

That Council

1. Notes and considers all submissions received in response to proposed Amendment C172.
2. Notes and endorses the Officer's response and recommendations to submissions as outlined in this report and in Attachment 1 (Submissions Report).
3. Endorses the recommended changes to proposed Amendment C172 as outlined in this report and Attachment 1 for the purpose of Council's position on the proposed amendment for the panel hearing.
4. Pursuant to Section 23(1) and Part 8 of the *Planning and Environment Act 1987*, request the Minister for Planning appoint an independent planning panel to consider the submissions received to proposed Amendment C172 to the Monash Planning Scheme.
5. Refers all submissions on proposed Amendment C172 to the planning panel appointed by the Minister for Planning.
6. Notifies all submitters of Council's resolutions on the proposed Amendment C172 as set out above.

INTRODUCTION

The purpose of this report is to consider the feedback received following the exhibition of Amendment C172 to the Monash Planning Scheme, which seeks to implement the Monash Boulevards Urban Design Framework (BUDF).

A report on the consultation, a summary of submissions received, and the officer response to submissions and recommended changes are set out in the Submissions Report (Attachment 1).

COUNCIL PLAN STRATEGIC OBJECTIVES

Sustainable City

Ensure an economically, socially, and environmentally sustainable municipality.

Prioritise sustainable transport options, including walking/ cycling paths and public transport.

Enhanced Places

Improve public spaces and local employment by revitalising our employment hubs, activity centres and neighbourhood shops.

Prioritisation of pedestrians and active transport over vehicles.

Pursue a planning framework that meets Monash needs.

BACKGROUND

The Monash Housing Strategy 2014

The Monash Housing Strategy adopted in 2014 contains a Residential Framework Plan that identifies eight distinct housing and built form areas. These give overall strategic direction to the level of change and dwelling intensity planned for each of these areas. The Monash Boulevards Urban Design Framework (BUDF) is a project that was identified in the *Monash Housing Strategy 2014*. The Monash Housing Strategy designates Dandenong Road/Princes Highway and Springvale Road as Category 4 – Boulevards, with the objective to provide for housing change and diversification along the boulevards.

The Monash Boulevards Urban Design Framework (2022)

The development of the Monash BUDF commenced with consultation on a Discussion Paper in August and September 2021. The submissions received indicated broad support for increasing the provision of housing and landscaping along the boulevards, and for many of the issues and opportunities outlined in the Discussion Paper.

Consultation on the Draft BUD was held in July and August 2022, with the final BUDF adopted by Council on 13 December 2022.

To implement the BUDF, proposed Amendment C172 was drafted. Council resolved on 26 April 2023 to request the Minister for Planning to authorise Council to prepare the Amendment C172, finalise the Amendment documentation and upon receiving authorisation from the Minister for Planning, to exhibit Amendment C172 in accordance with Section 19 of the *Planning and Environment Act 1987*.

DISCUSSION

Amendment C172 was exhibited from 31 October to 19 December 2024. The exhibition involved:

- Direct notification to all properties within the areas proposed to be rezoned as well as properties immediately adjacent.
- Notices placed in The Age newspaper and the Government Gazette.
- An article in the Monash Bulletin which was distributed in the week of 18 November 2024 to all households and businesses in the municipality.
- Copies of the draft planning scheme amendment and the adopted BUDF were provided in Clayton Library, the Oakleigh Service Centre and the Monash Civic Centre.
- Information about Amendment C172 was included on the Shape Monash page on Council's website. This included an interactive map where community members could find out what changes were proposed by property address.

Feedback was provided in the following ways:

- By completing a submission via the Shape Monash webpage.
- Emailing the Strategic Planning Department's inbox.
- One submission was made verbally via phone as the Submitter was unable to provide a written submission.

In response to the exhibition of the amendment, Council received 32 submissions. These submissions included a mix of support, requests for changes and objections.

A detailed report on the exhibition of the amendment, including officer responses to the issues raised in submissions, is provided in the Submissions Report at **Attachment 1**. Draft Design and Development Overlay schedules 6 and 19 showing supported changes are included as **Attachment 2**.

Key Issues Raised in Submissions

The main issues raised in submissions were:

- Potential impact of building heights
- Increase in density
- Impacts on neighbourhood character
- Vegetation and canopy cover
- Drafting issues

These issues are summarised and discussed below. Recommended changes are also provided in response to the submissions, where relevant.

Issue 1: Potential impact of building heights.

Several of the submissions raise concerns about the potential impact of higher building heights on areas adjacent to the properties to be rezoned. They especially raised the issues of loss of privacy, overshadowing, loss of views and increased visual bulk.

Issue 1 Officer response:

These comments are consistent with some of the feedback from the community when consulting on the draft BUDF.

Much of the land within the area to be rezoned is currently older single storey dwellings with some 2 storey development. Most of the land is within the General Residential Zone which has a current maximum height building limit of 3 storeys. The changes proposed by the amendment will increase the maximum height limit to most of the land involved to 13.5 meters (4 storeys). Only a few areas close to major intersections are proposed to have increased building heights of up to 21 metres (6 storeys), and 2277 Dandenong Road was proposed to have a height limit of 8 storeys or 27 metres.

It is recognised that the rezoning will enable a change in character and scale from what presently exists. However, these proposed changes are consistent with the directions of the Monash Housing Strategy and state policy of providing for increased housing supply and diversity in accessible locations. The overall increase in height by 1 to 3 storeys is considered modest and an appropriate response to the width and volume of Dandenong and Springvale Roads and their major intersections.

Schedule 6 to the Design and Development Overlay specifies that achieving heights of up to 4 storeys (in GRZ4 areas) will only be possible where the site width is 24 metres or greater. Taller

buildings up to 6 storeys would require a lot width of 30 metres or greater or where two or more sites will be consolidated. This calculation is based on the need to allow for adequate side and rear setbacks, adequate space for landscaping and protecting the amenity of surrounding residents through appropriate setbacks. Narrower sites will only be capable of developing up to 3 storeys, which is what is possible under the existing zonings.

The transition to a 4 and 6 storey height limit from adjoining single or double storey properties to the rear of the properties to be rezoned is adequately addressed within the DDO6 and DDO19 schedules. They include the requirement for built form to be setback off all boundaries, which exceeds what is required for residential development under ResCode (clauses 55 and 58).

Issue 1: Officer recommendation:

- No changes to proposed Amendment C172 are proposed in response to this issue.

Issue 2: Increase in density

Many submissions raised concerns that traffic along Dandenong and Springvale Roads and side streets will increase as a result of increased density along the boulevards.

The Department of Transport requested several drafting changes, that applications provide a transport impact assessment and to avoid vehicle access to Transport Zone 2 and 3 where possible.

Several submissions raised the concern that on-street parking would increase as a result of the increased density allowed by Amendment C172.

There was some concern raised by submitters of the extra burden to existing services and infrastructure caused by the increase in population density.

Issue 2 Officer Response:

Although the amendment seeks to provide opportunities for an increase in the density of dwellings along the Boulevards, this increase is unlikely to affect the current volume of traffic. Springvale and Dandenong Roads are arterial roads, which serve to accommodate very high levels of through traffic. They will be able to cope with a small increase in residential traffic generated by development along both boulevards.

It is anticipated that improvements to cycling and pedestrian infrastructure as well as the proposed new Suburban Rail Loop Stations will provide current and future residents with alternative choices to travel beyond driving.

Requiring a transport impact assessment to each permit application will ensure that due consideration is given to the effects of traffic by the proposed development. It can suggest mitigation measures to severe increases and accurately communicate any impacts to traffic to surrounding residents. Most major planning applications are already required to provide transport impact assessments, particularly if the application is required to be referred to Transport for Victoria under Clause 66.02-11.

Similarly, avoiding direct vehicle access to Dandenong and Springvale Roads will improve pedestrian safety and reduce potential collision points.

Council can adjust the restrictions and provision of on-street carparking supply based on local demand and circumstances.

Clause 52.06 of the Monash Planning Scheme requires that adequate carparking is provided on the land. For residential development, Clause 52.06 sets a rate of 1 car space for every 1-2 bedroom dwelling and 2 car spaces for every 3+ bedroom dwelling, and visitor car spaces of 1 to every 5 dwellings (if outside of the Principal Public Transport Network, if inside the PPTN there is no requirement for visitor spaces). The Amendment is not changing any of these rates through e.g. Parking Overlays. Council can manage on-street carparking supply by adjusting parking restrictions and limiting the number of resident parking permits.

Perceived extra wear and tear on physical infrastructure such as water pressure, sewage pipes, footpaths and roads as a result of population increase are the responsibility of the authorities that manage them (e.g. Yarra Valley Water and South East Water). It is common practice to adjust and maintain infrastructure based on project demand. The relevant service authorities were notified of the amendment and will be able to maintain and repair the infrastructure as needed.

Issue 2: Officer recommendation

- Revise Schedule 6 to the Design and Development Overlay to state that vehicle access to Transport Zone 2 should be avoided where possible and to provide only a single width crossover where possible.
- Revise Schedule 6 and 19 to the Design and Development Overlay to require a transport impact assessment for applications for non-accommodation buildings of three storeys or less and accommodation buildings of four storeys or more.

Issue 3: Impacts on neighbourhood character.

Many submitters were concerned with the impact of increased height limits and density to the existing neighbourhood character.

The existing character along the boulevards is of predominantly single storey dwellings on larger lots interspersed with light industrial and commercial areas. Submitters value their existing neighbourhood character for its traditional peaceful suburban living environment. It is a concern of residents that their current valued neighbourhood character will be destroyed with the development of apartment buildings contributing to their loss of privacy, visual bulk and blocking of their views.

Issue 3: Officer response

Although the Amendment does facilitate apartment development, the apartments will be no more than 4 storeys within most of the area affected by the Amendment, and 6 or 8 storeys within gateway areas and larger sites that can accommodate additional height without impacting detrimentally on neighbourhood character.

It is the intention of Schedules 6 and 19 to the Design and Development Overlay to minimise visual bulk by requiring larger setbacks than would otherwise be required by ResCode. Additionally, the requirement for strongly landscaped areas and new tree planting will screen new medium density development from the public and private realms.

Issue 3: Officer recommendation

- No changes are proposed in response to this issue.

Issue 4: Vegetation and canopy coverage

Several submissions advocate for further planting of trees and vegetation as well as ensuring adequate setbacks for the provision of trees on private property. Conversely, some submitters are concerned that the increase in height and density will result in the destruction of existing trees.

One submitter requests that Plane and Sycamore trees be required to be planted.

Other submitters argue that the requirement of the draft ordinances to provide adequate space for tree planting and the provision of canopy trees is burdensome and reduces the opportunity for development.

Issue 4: Officer response

Amendment C172 and the BUDF seek to provide consistent and strongly landscaped setbacks along Springvale Roads and Dandenong Roads. Schedule 6 to the Design and Development Overlay provides requirements to ensure denser planting of canopy trees along the Boulevards through the 7.6 metre Boulevard Setback and generous side and rear setbacks by maximising deep soil planting zones and requiring that 60 percent of the total area of the boulevard interface incorporate grassed and planted areas.

Street trees are Council assets and require permission to be removed. In terms of protection of trees on private property, it is out of scope of the Amendment to implement tree protection controls, although there is an existing local policy (Clause 15.01-1L-02) that is designed to encourage tree retention.

Issue 4: Officer recommendation

- No changes are proposed in response to this issue.

Issue 5: Drafting of the schedules to the Zones and Overlays.

Several submitters raised issues with the drafting of the schedules to the Residential Growth Zone, General Residential Zone and Design and Development Overlay. The submitters disputed the boulevard setback, increased side and rear setbacks and permeability standards stating that they were too restrictive.

Submitters of larger sites stated that the maximum height limit was too low and that the increased requirements intended to protect the amenity of surrounding occupiers limited their development potential.

One submitter asked whether the building heights would be mandatory or discretionary.

One submitter noted that the maps should be of higher quality to avoid “blurriness” when zoomed in.

Issue 5: Officer response

The new maximum height limits for the Boulevards were developed with consideration to their site context. The application of 4 storey height limits are appropriate along the boulevards where they respond to the existing low-scale residential surrounds and the absence of major transport and retail land uses in the area. 6 storey height limits for a small number of sites reflect the building heights identified in nearby precinct plans, activity centres and close proximity to shops, services and transport. It would be inappropriate to raise the maximum building heights on sites to allow buildings that do not adequately respond to the existing nearby infrastructure.

Similarly, reducing the boulevard, side and rear setbacks to accommodate larger development undermines the intention of the proposed Clauses. All new Clauses seek to provide a consistent, strongly landscaped setback to enhance the garden city character of Monash. The 7.6 metre Boulevard setback is intended to enhance the 'Boulevard Character'. 7.6 metre front setback is required at a minimum to ensure planted trees will survive and have sufficient room for large canopy coverage and has been a consistent setback applied in Monsh for many years and is the front setback of the current zone (GRZ2). The wider side and rear setbacks have the dual purpose of mitigating amenity impacts of overlooking, overshadowing and visual bulk to surrounding properties and providing for the consistent landscaped setting the amendment is seeking. The submissions that call to reduce the setbacks, permeability or height limit within the Schedules to the Residential Growth Zone, General Residential Zone or Design and Development Overlay are not supported.

The proposed Residential Growth Zone has a discretionary height limit as required by a condition of the authorisation. Design and Development Overlay Schedule 6 has a preferred maximum height limit of 21 metres or 6 storeys and a decision guideline that states that where there are inconsistencies between permit requirements of the Residential Growth Zone and Design and Development Overlay Schedule, that the overlay will prevail. This decision guideline should be reworded to be less ambiguous.

One submission raised an issue with the drafting of the requirement in Schedule 6 to the Design and Development Overlay to "*Allow for the interpretation or reading of each floor level of the building*", stating that it was ambiguous and too prescriptive. It is agreed that there are other requirements in the schedule that adequately relate to the design of buildings and that this requirement is not required.

Not all the drafting changes requested by the submitters are supported, however the changes requested below are considered reasonable.

Issue 5: Officer recommendation

- Delete the requirement of "*Allow for the interpretation or reading of each floor level of the building*" from Schedule 6 to the Design and Development Overlay.
- Rewrite the last decision guideline within the Schedule 6 to the Design and Development Overlay to be clearer that building heights in Design and Development Overlay 6 prevail over the discretionary heights of Schedules 4 and 7 of the Residential Growth Zone.
- Seek advice from Planning Panels Victoria as to whether the maximum height limit should be a mandatory height limit within Schedule 7 to the Residential Growth Zone or remain as a discretionary height limit within Schedule 6 to the Design and Development Overlay.

FINANCIAL IMPLICATIONS

There are no financial implications to this report.

POLICY IMPLICATIONS

There are no policy implications to this report.

CONSULTATION

A detailed report on consultation is included in the Submissions Report (**Attachment 1**).

Planning Scheme Amendment C172 which seeks to implement the Monash Boulevards Urban Design Framework was exhibited in accordance with the requirements of the Planning and Environment Act 1987. Over 2,000 letters were sent out to affected owners and occupiers. 32 submissions have been received in response to the exhibition.

The exhibition for Amendment C172 under Section 19 of the *Planning and Environment Act 1987* has now concluded.

SOCIAL IMPLICATIONS

The Amendment will have many positive social impacts for the Boulevards. It seeks to enhance social sustainability through creating a sense of community along the Boulevards and facilitate housing growth, specifically apartment developments. Increasing housing diversity in Monash will allow people to stay connected to their existing communities and welcome new members.

HUMAN RIGHTS CONSIDERATIONS

There are no human rights implications to this report.

GENDER IMPACT ASSESSMENT

A GIA was not completed because this agenda item is not a 'policy', 'program' or 'service'.

CONCLUSION

Planning Scheme Amendment C172 which seeks to implement the Monash Boulevards Urban Design Framework has now completed its formal exhibition process.

32 submissions have been received in response to the exhibition of the amendment. The most common issue raised in submissions related to the proposed building heights. Other key issues included increased density, impact to neighbourhood character, increased traffic and on-street carparking, increase to vegetation and the drafting of the proposed ordinances.

Given there are objecting submissions that are unable to be resolved, it is recommended that Council request the Minister for Planning to appoint an independent planning panel to consider the Amendment and all submissions.

ATTACHMENT LIST

1. Submissions Report C172 [**7.1.4.1** - 28 pages]
2. Draft Design and Development Overlay Schedules showing changes [**7.1.4.2** - 16 pages]

Amendment C172 - Submissions Report

PROJECT BACKGROUND

Amendment C172 proposes to implement the changes to the Monash Planning Scheme recommended by the Monash Boulevards Urban Design Framework (BUDF). The BUDF is a project identified in the Monash Housing Strategy 2014. The BUDF provides a vision and framework for built form changes along the Boulevards (Dandenong Road and Springvale Roads) and urban realm improvements.

The amendment was authorised by the Minister for Planning on 12 July 2024 subject to 13 conditions. At the Council meeting on the 27 August 2024 Council resolved to exhibit the amendment in accordance with Section 19 of the *Planning and Environment Act 1987* (the 'Act'). 32 submissions were received. Each of the submissions are summarised and responded to in Appendix 1 of this report.

INTRODUCTION

Amendment C172 was exhibited in accordance with Section 19 of the *Planning and Environment Act 1987*.

The purpose of exhibiting Amendment C172 was to meet the statutory requirements of The Act by consulting primarily with affected and surrounding owners and occupiers as well as the wider Monash community about the proposed changes – i.e. to rezone properties adjoining Dandenong and Springvale Roads and introduce a Design and Development Overlay (Schedule 6).

Exhibition was conducted from 31 October to 9 December 2024.

Section 19 of The Act requires that a planning authority must give notice of the preparation of the amendment to every Minister, public authority, council owner and occupier of land that may be materially affected by the amendment and prescribed ministers, public authority and municipal councils.

Letters to those described above were sent from 31 October 2024. A Shape Monash Webpage was available for the duration of the exhibition period. Notices of Amendment C173 were included in The Age on 5 November 2024, and the Government Gazette on 7 November 2024. Non-statutory consultation was also undertaken. An article appeared in the Monash Bulletin, which was circulated from 18 November 2024. The community were able to make written submissions via the Shape Monash website, email, mail or in person.

Planning Scheme Amendments normally involve a 'consult' form of engagement in accordance with the IAP2 criteria, particularly when the community has had previous ability to influence the strategic documents that underpin the amendment (e.g. the BUDF).

SUMMARY OF ENGAGEMENT

Methods used	Stakeholders	Dates
Direct Notification: Letter drop	Affected residents, prescribed ministers, adjoining municipality councils, other stakeholders	31 October 2024
Shape Monash Webpage	All stakeholders	31 October 2024 – 9 December 2024
Notice in The Age newspaper	All stakeholders	5 November 2024
Notice in the Government Gazette	All stakeholders	7 November 2024
The Monash Bulletin	All residents and businesses	18 November 2024
Copies of the proposed Amendment were provided in the Glen Waverly Civic Centre, Clayton Library and Oakleigh Service Centre	All residents and stakeholders	31 October – 9 December 2024
Phone calls and counter enquiries	A Strategic Planning Officer was available to take calls and receive counter enquiries for the duration of the exhibition period.	31 October – 9 December 2024

ENGAGEMENT RESULTS

Summary of Feedback

A total of 32 submissions were received.

- 7 from owners/occupiers of affected properties.
- 16 from owners/occupiers of surrounding properties.
- 2 from neighbouring Councils
- 3 from government departments/agencies
- 2 from public utility companies
- 1 from visitors/workers
- 2 Anonymous¹

¹ The ability for someone to submit a submission anonymously through Shape Monash was discovered very late in the process.

Submissions from individuals

- Within or near a precinct along Dandenong Road: 13
- Within or near a precinct along Springvale Road: 7
- Other areas: 7

Submissions from non-individuals /organisations

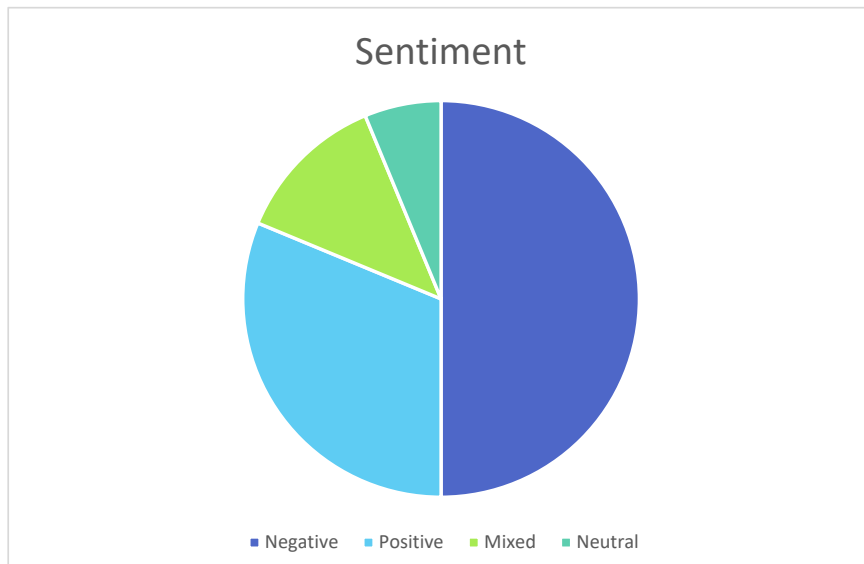
The organisations that provided a submission are:

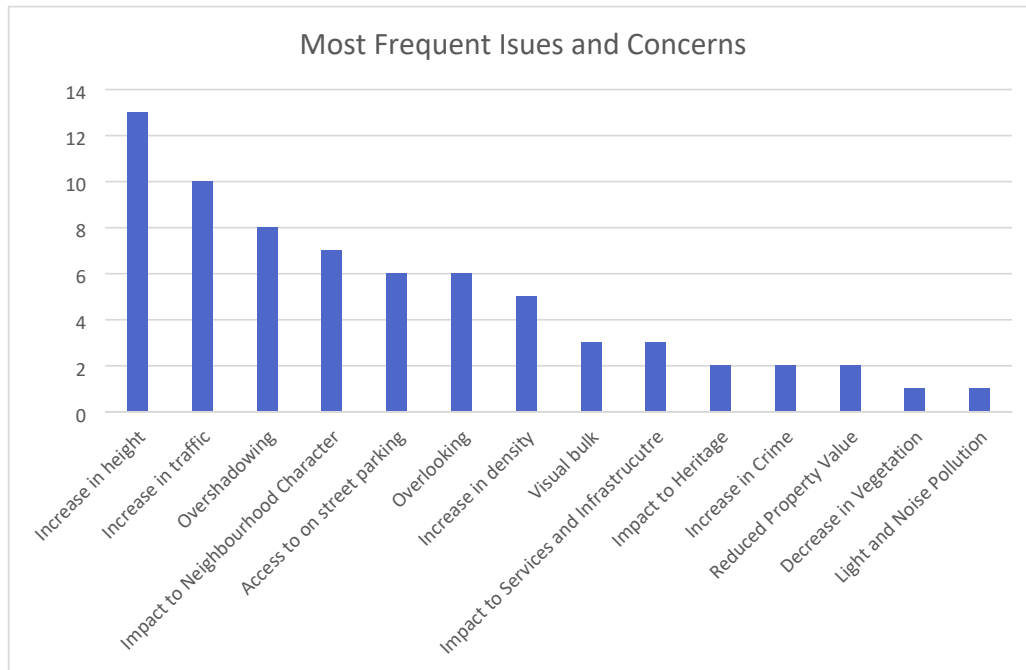
- The City of Whitehorse
- The City of Greater Dandenong
- Department of Transport and Planning
- APA Group
- Melbourne Water

Note: The submissions analysed in this report include 3 late submissions provided between the closing date of submissions and 25 February 2025.

Snapshot of feedback themes

Below is a snapshot of overall sentiment:





APPENDICES

Appendix 1: Full summary of stakeholder responses

Summary of stakeholder responses (table format)

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
SUB01 – Resident in Clayton	Planning controls should include adequate setbacks or other design methods to provide for planting and canopy trees along thoroughfare frontages.	Neutral	<p>The planning controls will require a 7.6 metre front setback from the boulevard where 60% should be occupied by landscaping.</p> <p>The proposed DDO6 requires upper levels above 13.5 metres should be setback an additional 3 metres from the boulevard boundary (10.6 metres in total).</p> <p>The setbacks required by the DDO6 will be sufficient to allow prominent planting and landscaping along the boulevard thoroughfare frontages.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB02 – Anonymous	<p>Broadly supports amendment.</p> <p>Advocates for more trees and planting and cycling infrastructure.</p> <p>Questions the plans for cycling infrastructure along Springvale Road where there are no service roads.</p> <p>Commentary on the state of cycling infrastructure in Melbourne. Suggestion to make side streets more cycle friendly and requests that this be done in this Amendment.</p> <p>Identifies that painted sharrows cycle paths should not be where parking is permitted otherwise it will not be safe.</p>	Positive	<p>More planting and greening is supported and there are provisions in the amendment to encourage this, including in DDO6.</p> <p>Infrastructure provision is not part of the amendment.</p> <p>Cycling improvements along both boulevards in line with the BUDF would occur based on need and in line with our normal budgetary processes.</p> <p>The Amendment does not include land within the Glen Waverley Major Activity Centre (SR2), where there is a noticeable absence of service lanes along Springvale Road.</p> <p>Commentary on cycling infrastructure in Melbourne is noted.</p> <p>The request to implement cycling infrastructure in local streets is out of scope of the amendment.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB03 – Resident adjoining DR2	The Submitter states that the proposed height limits are a gross over development. The Heritage Overlays must be respected and not lost through the amendment and	Negative	There is a modest increase of one storey in height limits near where the submitter lives. It is not considered to be an overdevelopment of the area, particularly considering the width of Dandenong Road.

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
	<p>development process. The proposed setbacks will not protect the valued neighbourhood character or heritage values. Cyclists and pedestrians are being prioritised over the community.</p> <p>All supporting maps should have a higher resolution to enable closer examination.</p>		<p>Heritage Overlays are applied to only four properties in the amendment (1436A, 1650-1656 and 2277-2251 Dandenong Road and 125 Springvale Road), and these are being retained without changes. Any development to a property within a Heritage Overlay must be assessed against the decision guidelines of the Heritage Overlay independently of the introduced Design and Development Overlay and new zones.</p> <p>There are some properties within HO92 along Dandenong Road but they are not being rezoned by the amendment and will retain their NRZ1 zoning. The proposed setbacks in Schedule 6 to the Design and Development Overlay are intended to ensure the height and built form of new buildings provide an acceptable interface, amenity outcomes and transition to adjoining lower scale residential areas. The setback requirements are intended to protect the amenity of all neighbouring properties, including those affected by the Heritage Overlay. The Heritage Overlay itself contains stronger provisions to protect the heritage buildings it affects. It is the correct tool for the protection of Heritage. The Design and Development Overlay setback requirements work to protect the amenity of neighbouring properties by setting back visual bulk, protecting their access to sunlight and limiting overshadowing.</p> <p>Cycling and pedestrian infrastructure is outside the scope of the planning scheme amendment. However, the BUDF recognises the importance of improving access to cycle paths and better pedestrian infrastructure to provide choice for future residents and improve health outcomes.</p>
SUB04, Resident in DR4	<p>Questions how additional traffic, demand for on-street parking and convenient and safe pedestrian movement will be addressed. Concern for additional pressures on waste collection.</p>	Negative	<p>A general response acknowledging the submission was emailed to the submitter. The questions were rhetorical of nature, intended to bring Council's attention to these issues which were not adequately explained in the Amendment Documentation.</p>

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
	<p>Questions also how privacy will be achieved in a landscape of up to 6 storey buildings.</p> <p>States that there was no previous notice or communication from Council about the BUDF. Questions how the BUDF implementation was able to proceed without community engagement.</p>		<p>Springvale and Dandenong Roads are major arterial roads which will be able to accommodate any additional traffic. Whilst private car use will always be an available option, the improvements to pedestrian and cycling infrastructure as well as the implementation of the Suburban Rail Loop Stations of Glen Waverly, Clayton and Monash will allow for a more connected public transport system reducing cars on the roads. See response to Submission 8 regarding amenity impacts to overlooking concerns.</p> <p>A response was sent to the submitter via email outlining previous consultation with the community on the Draft BUDF in July-August 2022.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB05, Resident in SR1	Supports Amendment. Administration Questions regarding when the amendment will commence and if any further action is required by the Submitter.	Positive	<p>Noted.</p> <p>A response outlining the next stages of the amendment process was emailed to the submitter.</p>
SUB06 City of Whitehorse	City of Whitehorse's formal response. No objections or concerns listed.	Neutral	Noted.
SUB07	Supportive with requested changes. Requests higher density of 6 storeys to replace 4 storey areas and a lesser setback of 3 or 4 metres for properties fronting Dandenong Road. This is noted in particular in areas close to parklands and sports ovals. Concern the 7.6 metre setback will encourage higher development to the rear of sites, overshadowing properties to the rear.	Positive	<p>There was significant concern raised in the engagement for the BUDF where building heights of more than 4 storeys would be visually prominent in some locations and detract from the amenity of the area. Subsequently, the height limits of 6 storeys were reduced to 4 storeys in these locations to accommodate the community's concern.</p> <p>Similarly, the height limit of deeper lots were restricted after the first 50 metres from the Boulevards. After 50 metres, the height will be a maximum of 4 storeys. This is intended to mitigate against visual bulk and focus heights along the boulevard interfaces.</p>

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
	Notes the proposed height limits are inconsistent with the State Governments Housing targets to provide for 72,000 new dwellings within Monash.		<p>The front setback requirement of 7.6 metres from the Boulevards is intended to realise the vision of the BUDF “A leafy landscape outlook will dominate views along the boulevards, strengthened with additional tree and understorey planting... Front gardens will further strengthen the landscape dominated environment” by the design objective “To provide opportunities for deep soil zones and canopy trees within front setbacks”. While a 3-4 metre front setback would allow more area for development, it would come at the cost of the consistent landscaped boulevard setback that the amendment is seeking.</p> <p>The amendment will remove the current height limit of 3 storeys (11 metres) to allow development of 4, 6 and 8 storeys (13.5m, 21m and 27m respectively).</p> <p>The 4 storey height limit is the balance between achieving higher density and building more homes whilst achieving the desired Boulevard Character in the BUDF. Council is currently undertaking a Residential Capacity Analysis (RCA) to assess Council’s current and future supply/demand of housing. While not yet finalised, the preliminary data indicates Council has capacity to exceed the Victorian Governments requirement for 72,000 new dwellings by 2051.</p> <p>Increasing density next to parklands and ovals is out of scope of this amendment.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB08, Resident adjoining DR2	Submitter objects to the entirety of the amendment. Concern over the impacts of increased density to privacy, property value, overlooking and general amenity. Concerns the proposed re-zoning will increase the noise, traffic, general disruption, destruction	Negative	Assessment of any new development against Clauses 55 and 58 of the Monash Planning Scheme will mitigate the amenity impacts of overshadowing, noise and overlooking. Further, the DDO6 requires upper floor setbacks greater than what is required by ResCode. The amendment contains the neighbourhood character objective in the GRZ4 and the design objective in RGZ7 to ensure buildings are within a strongly landscaped

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
	<p>of trees and greenery and will not be sympathetic to the existing neighbourhood character.</p> <p>The rezoning will undermine the Heritage Overlay and strain the infrastructure. This will result in pressures on traffic and parking.</p>		<p>setting. The policy specifies vegetation to be planted within the front and side setbacks including vertical greening and green roofs.</p> <p>The Heritage Overlay is intended to protect areas of existing heritage value and no changes are proposed to the Heritage Overlays as part of this amendment. Therefore heritage overlays will continue to protect places of heritage value.</p> <p>Although the existing character is of detached, single storey dwellings, the new medium density development will be well designed with adequate setbacks and greenery to respect the existing character whilst providing more housing opportunities. The Design and Development Overlay – Schedule 6 intends to enhance the garden city character of Monash by ensuring adequate setbacks of ground and upper floors from neighbouring properties.</p> <p>The provision of more public and active transport opportunities to the Boulevards will reduce the reliance on cars and create a walkable community. It is therefore envisaged that traffic will become less congested, despite the Boulevards accommodating more people. See response to Submission 9.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB09, Resident adjoining DR6.	<p>Objection to the Amendment.</p> <p>Identifies multiple vehicle accidents occur on these roads. Links the increase in density to increases in crime, traffic congestion and vehicle accidents.</p> <p>States that it is Council’s responsibility to ensure that changes that will affect the existing environment of families do not occur.</p>	Negative	<p>Almost all of the areas along the boulevards are within the Principal Public Transport Network (PPTN). In addition, there are new proposed train stations as part of the Suburban Rail Loop that are within proximity to some parts of the boulevards and this will increase local access to public transport.</p> <p>The majority of traffic on Springvale and Dandenong Roads is through traffic, rather than residential traffic. Therefore, traffic congestion is unlikely to increase. There is nothing Council can do to reduce the traffic in this</p>

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			<p>area, and marginally increasing potential traffic through increased density would make a negligible (and unnoticeable) difference.</p> <p>Council also has a responsibility to accommodate future population growth, with the State Government recently announcing housing targets for all Victorian municipalities.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB10 Resident adjoining DR6	<p>Objects to the amendment. The increase of height to 6 storeys will require more on-street parking spaces. The traffic will increase. It will increase crime and overshadow existing dwellings including solar panels resulting in higher electricity bills.</p>	Negative	<p>See response to Submission 9 regarding increases in traffic and on-street carparking. Provisions for parking must be accommodated on site in accordance with Clause 52.06. Otherwise, with improvements to public transport systems and cycling infrastructure, active transport will be a competitive option for current and future residents.</p> <p>There are no demonstrated links between increased housing densities and increased crime rates, nor has the submitter provided any. Higher densities when done well can reduce crime through increased visual surveillance of the street and public areas.</p> <p>The design requirements of proposed Schedule 6 to the Design and Development Overlay, and the variations to ResCode in the zone schedules intend to preserve the amenity of existing and future residents through adequate side, rear and upper level setbacks.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB11 Resident in SR4	<p>Rezoning adjacent property of 525-535 Springvale Road to allow 21 metres in height would result in overshadowing.</p>	Negative	<p>The submitter's property is located to the north-west of properties proposed to be rezoned with no increased heights to be built north of the resident's property. There will be minimal overshadowing or reduced solar access to north facing windows.</p> <p>The side, rear and upper level setback requirements in DDO6 will seek to minimise overshadowing to adjoining properties.</p>

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			No changes are recommended to the amendment in response to this submission.
SUB12 Resident in Hughesdale, Parts 1&2	Plant several large Sycamore and Plane Trees.	Neutral	<p>The BUDF and amendment seek to facilitate planting of more canopy trees in the public and private realms. Specifying what type of trees should be planted is out of scope of the amendment. Council has an adopted Tree Management Plan, which sets out appropriate species and planting guidelines for new street trees.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB13, Resident adjoining SR3 (Verbal submission taken over the phone as submitter was unable to provide written submission)	The proposed increases in height limit will encourage denser development which will not be keeping with the existing character of Glen Waverly. Council's work should be about protecting the lifestyle and amenity of existing residents.	Negative	<p>The purpose of the amendment is to implement the Monash Housing Strategy as it applies to the boulevards (Category 4) and the BUDF, allowing for development in appropriate areas to meet the forecast demand for housing in Monash.</p> <p>Planning for and facilitating population growth is a core part of Council's work. The amendment strikes a balance between protecting the amenity of existing residents whilst planning for future residents and development.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB14, Resident in Wheelers Hill.	Objects to the proposed 4 and 6 storey height limits. Raises issues regarding on-street carparking, objection to basement carparking. Raises concern that submissions will not be viewed.	Negative	<p>Clause 52.06 of the Monash Planning Scheme sets the requirements for carparking and requires that adequate carparking be provided on the developed land. Council can control the availability of on-street car parking by reviewing, adjusting and enforcing parking restrictions and issuing resident parking permits.</p> <p>The Planning and Environment Act 1987 requires Council as the Planning Authority to consider and respond to each submission received. Where Council does not change the amendment to satisfy a submission, it must be</p>

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			<p>referred to an independent planning panel should Council wish to proceed with the amendment.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB15, Resident in SR5	<p>Supports higher density housing along main roads in municipality. Raises awareness of how corner sites and easements are restrictive to future development. A marked up version of the draft GRZ4 and DDO6 contain queries and suggestions to the drafting. They are grouped by planning control, interpreted to the best of the Officer's ability and responded to below.</p>	Positive	<p>The amendment seeks a minor increase to the side street setback from 2m to 3m, affecting corner sites but this is unlikely to detrimentally impact on dwelling yield. However, the intention is to encourage consolidation with excellent design.</p> <p>It is recognised that easements are limiting. As they are to the benefit of other authorities they cannot be removed easily and are a constraint of the land. However, if the infrastructure protected by the easements is moved, the easements can be removed or relocated.</p> <p>No changes are recommended to the amendment in response to this part of the submission.</p>
SUB15 (continued)	<p><u>Queries relating to the drafting of the GRZ4:</u></p> <p>Must the reader refer to the DDO6 when seeking to develop up to 4 storeys on consolidated lots?</p> <p>There shouldn't be any inconsistency between the requirements of the DDO6 and the GRZ4.</p>		<p>The DDO6 must be referred to when designing four storey developments. Preferably there won't be any inconsistency but when the same tool seeking the same objectives is placed over two different character areas, the decision guidelines sought by the DDO6 should prevail as it is the more nuanced control seeking more specialised outcomes.</p> <p>Recommendation – redraft wording to clarify that the DDO building heights prevail.</p>

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
	Why was the permeability standard increased?		The permeability standard has been increased to 30% (in the schedules to the residential zones) to account for the strengthened landscaping requirements of the BUDF. The BUDF guidelines for Landscape Design require that "the front setback should incorporate grassed and planted areas comprising a minimum of 60 per cent of the total area." A higher percent of permeable area (more than the default 20%) will be required to achieve this guideline.
	How do the increased setbacks allow developments on corner sites?		The small increase to the setback to side streets (3m compared with the default 2m) on corner sites are to encourage greater visual separation and landscaping.
	How is the maximum height limit resolved on sloping sites?		The parent provision of the General Residential Zone allows the maximum height limit to be exceeded if it is a sloping site. There is no need to cover this in the schedule to the zone.
SUB15 (continued)	<p><u>Queries relating to the drafting of the DDO6.</u></p> <p>Definitions of low rise, mid rise and high rise should be revised to be: 1-4 stories is low rise, 5-14 storeys is mid rise and 15+ storeys is high rise.</p>		The DDO's interpretation of medium rise is consistent with the definition provided by the Australian Bureau of Statistics ² .
	Suggests separating Objective 4 into two points. Notes that setbacks are just one component of providing overall amenity.		Not Supported. The Ministerial Direction on Form and Content requires a maximum of five objectives in an overlay schedule. Upon review of the objectives, officers consider the exhibited wording to be the most appropriate.

² From the website "Telling storeys – apartment building heights" Australian Bureau of Statistics (2020)

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	<p>Suggests inserting “within 3 metres of” to ‘a permit is required to construct a fence within 3 metres of any street interface’.</p>		<p>Not supported. A permit is only required to construct a front fence if it’s associated with one dwelling on a lot less than 300 square metres and the front fence exceeds the maximum height in Clause 54.06-2. The draft wording of the DDO6 requires a permit for any fence constructed within the street interface for any development. The boulevard setback is not always the front setback. Standards A20 and B32 only apply to front fences, not side fences nor to non-residential developments (such as medical centres). Specifying fence requirements is now consistent along the Boulevard interfaces.</p>
	<p>The wording of Table 1: Building height and lot width is clunky but discretionary. Suggests that the height should be 13.5 metres up to four storeys unless it is more than 50 metres from the boulevard boundary.</p>		<p>Not supported. There is no need to state something in the table that is clearly articulated in the paragraphs or dot points above.</p>
	<p>Basements should be allowed to encroach within the boulevard boundary. 7.6 metre setback is a bit much. Usually 4 metre setbacks are sufficient for canopy tree planting.</p>		<p>Basements should be discouraged within the boulevard setback, however, it is possible for a permit to be issued to allow basements in this setback depending on its design. The 7.6 metre setback allows more depth for tree planting and greenery to provide a consistent, strongly landscaped boulevard setback enhancing the garden city character of Monash.</p> <p>This request is not supported.</p>
	<p>Dot point 4 of the Boulevard Setback requiring sufficient canopy spread to shade at least 80 percent of the boulevard setback is too prescriptive. It doesn’t account for deciduous trees or the time of year.</p>		<p>The provision provides enough guidance and detail to achieve the objective to provide a consistent, strongly landscaped setback along each boulevard, enhancing the garden city character of Monash.</p> <p>No changes are proposed as a result of this comment.</p>

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
	Conflicting points at dot points 1 and 3 of Other Setbacks. It is unclear whether the development should provide a 3 metre front setback to the side street, or the existing side streets should be respected.		Dot point 1 relates to the minimum setback of the secondary frontage of corner sites. Dot point 2 relates to respecting the existing setbacks of the side street if they are setback more than 3 metres requiring that side setbacks should respect the existing setback pattern of the street. If no existing setback pattern the setback should be at least 3 metres. No changes are supported in response to this issue.
	Seeks wall height before upper level setbacks to be 11 metres rather than 10.1 metres exhibited.		Not supported. 10.1 metres is an appropriate wall height above which upper level setbacks occur. No changes are requested. Cannot be sure of the issue raised.
	Questions the need to provide a setback of 4.5 metres to the common boundary to accommodate access to sunlight for habitable room windows, balconies and terraces, decks, patios on the ground of first floor.		For general amenity and views, it is appropriate to provide a 4.5 metre setback on the ground and first levels.
	Exhibited Wording was to “Allow for the interpretation or reading of each floor level of the building”. Diminish sense of scale by blurring this statement.		It is unclear how blurring the floor levels will result in a diminished sense of scale. However, this requirement may be unclear and there is adequate design guidance provided by other statements in the DDO that require sufficiently articulated visual bulk, therefore making this point redundant. It is recommended that this requirement be deleted from the DDO6.
	Questions whether the clause relating to utilising alternate materials for privacy of open spaces is allowed.		The wording of the clause indicates that alternative materials should be used. No changes are requested by the submitter in this statement.

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	<p>Suggests additional wording to ensure that deep soil planting zones will not compromise the development potential.</p>		<p>Not supported. Ensuring that the deep soil planting zones will not compromise the development potential does not align with the design objective of the DDO6.</p> <hr/> <p>Changes are recommended to Amendment C172 in response to this submission to:</p> <ul style="list-style-type: none"> • Delete the requirement of “Allow for the interpretation or reading of each floor level of the building”. • Separate Objective 4 into two dot points: 1. <i>To encourage lot consolidation in order to achieve the maximum building heights.</i> 2. <i>To provide for sufficient building setbacks to deliver high levels of internal and external amenity.</i>
<p>SUB16, Resident nearby DR4</p>	<p>Submitter is a cyclist and appreciates the focus on cycling. Raises concern about the increase in traffic on service streets resulting in dangerous traffic conditions for cycling.</p>	<p>Positive</p>	<p>The service lanes of major arterial roads are capable of accommodating increases in traffic.</p> <p>Noted. The BUDF will advocate for lower speeds on service roads and driver awareness to improve safety for cyclists. The BUDF will also investigate the potential for line marking of parking bays to delineate the carriageway and improve safety for all road users. The BUDF identifies some places where cycle cut-throughs should be installed, in order to reduce the need for cyclists to traverse into traffic. There are cycle and shared paths outside of the arterial road environment, including some separated bike paths.</p> <p>There will potentially be actions and further engagement on cycling infrastructure in the future.</p>

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			No changes are recommended to the amendment in response to this submission.
SUB17, Anonymous	<p>Raises concern over the treatment of the street with future high density development. Also contends that the infrastructure must keep up with higher density development. Concerns over existing crowded and slow traffic conditions on Springvale, Blackburn and Clayton Roads and future pressure to traffic. Questions what actions will take place to alleviate this.</p>	Mixed	<p>The BUDF contains the following actions to create a positive street environment:</p> <ul style="list-style-type: none"> - Improve amenity at crossings on service roads, specifically at bus stops, including canopy planting, kerb outstands, vehicle speed reduction and traffic island upgrades. - Consider high volume crossings as key opportunities for street furniture, respite, amenity and social interaction. <p>The engagement process is designed for the community to contribute to a positive urban environment together with Council.</p> <p>Springvale, Blackburn and Clayton Roads are all main arterial roads, which serve to accommodate high levels of through traffic. They will be able to cope with a small increase in resident traffic generated by development along both boulevards. Additionally, the BUDF and the Clayton, Monash and Glen Waverly Suburban Rail Loop Stations will promote public transport and cycling use thereby reducing the number of cars on these roads.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB18, Employed in Mount Waverly	Supportive. Requests an increase in height limit to 5 storeys for adjoining lots in DR4.	Positive/Change	<p>Not supported.</p> <p>Adjoining lots are encouraged to consolidate to enjoy the maximum benefit of the proposed height limit (4 stories in DR4).</p> <p>The draft GRZ4 contains the objectives to facilitate the development of apartment developments up to four storeys on consolidated lots. The results of previous community consultation reduced the initial proposed height limit from 6 storeys to 4 storeys to mitigate concerns of visual prominence raised by submitters.</p> <p>No changes are recommended to the amendment in response to this submission.</p>

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
SUB19, Consultant on behalf of Landowner in SR55	The submission states that the amendment does not respond to the residential development opportunity of their client's site and requests a review to provide for more residential opportunity for 682-688 Springvale Road, Mulgrave. Supporting statements within the BUDF are drawn upon to assist in their argument that the site can be developed further.	Mixed	<p>Not supported.</p> <p>Precinct SR05 is not identified as a key development site nor as a gateway site within the BUDF. The amendment proposed an increase in maximum height from the current three storeys to four storeys.</p> <p>The BUDF does not identify the site as a housing opportunity within Section 2.11. Rather, it is suitable for 'additional housing opportunities' through building heights of up to 4 storeys. This site has not been singled out as a site for increased housing densities despite being a corner site with two frontages.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB20, Resident adjoining DR2	The submission lists their concern under the general terms of overshadowing, increase in traffic and loss of amenity, then goes on to suggest the closure of their one way residential street to create a pocket park and altering the parking restrictions to 24 hour parking.	Negative	<p>The proposed height limit is 4 storeys or 13.5 metres high, 1 storey or 2.5 metres higher than currently allowed. 4 storeys are reasonable in the context of the surrounding maximum 3 storey built form in residential areas. See response to Submission 10.</p> <p>Changes to Westley Street are out of scope of this amendment.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB21, Resident Adjoining DR4	Submission opposes the amendment, in particular rezoning Gateway location at 1717-1737 Dandenong Road, Oakleigh East. Questions why this site was identified as a Gateway site. Cites amenity impacts of overlooking, and loss of on-street carparking.	Negative	<p>The property is adjacent to DR4, which is proposed to be rezoned to RGZ7, allowing development up to 6 storeys / 21m.</p> <p>Gateway sites are located at key road intersections. They are intended to be nodes of activity creating identifiable neighbourhoods with a strong urban character.</p> <p>The DDO6 will minimise the impacts of overlooking, overshadowing and visual bulk to surrounding properties by requiring larger side and rear setbacks than what ResCode would usually require.</p>

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			<p>Parking is required to be provided on site in accordance with Clause 52.06. Council can manage on-street parking supply by adjusting parking restrictions, introducing paid parking and limiting the number of resident parking permits. Public roads are a public good and facilitate movement of people.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB22, Resident in DR4	<p>The submitter supports strengthening the garden city character and expresses hope to achieve it using high density. However, the submitter expresses concern about the proposed 4 storey height limit impacting neighbouring properties via overlooking, overshadowing, views, and loss of on-street parking. Expresses the need for balance between the loss of amenity of existing residents to allow development for future residents.</p>	Mixed	<p>The proposed new height limits implement the vision and built form requirements of the BUDF. The BUDF has been through two stages of public consultation and was adopted by Council in December 2022.</p> <p>The DDO6 ensures adequate setbacks and encourages lot consolidation which will minimise overshadowing and overlooking. Sky views are not protected under the Monash Planning Scheme and are not considered a right to existing residents. See response to Submission 10 and 21 regarding protecting the existing amenity of residents.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB23, Resident adjoining DR6	<p>Submitter objects to the amendment on the grounds of increases in traffic, specifically to the ingress and egress to Dandenong Road along one way service lanes. The submitter also objects around the proposed maximum building height of 4 stories causing overshadowing and overlooking. Suggests the solution of a rear boundary setback or widening Roberts Avenue.</p>		<p>See response to Submission 9 in regard to the increase in traffic. It is out of scope of the amendment to widen local roads.</p> <p>See response to Submission 10 and 21 regarding protecting the amenity of existing residents.</p> <p>No changes are recommended to the amendment in response to this submission.</p>

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SUB24, City of Greater Dandenong	Supporting submission from the City of Greater Dandenong.	Positive	No changes are sought or are recommended to the amendment in response to this submission.
SUB25, Department of Transport and Planning	<p>Submission from Transport Victoria. Seeks several drafting changes to the DDO6 and DDO9 as well as the BUDF to better reflect Transport Victoria’s strategic documents and request further assessments. The submission also states that raised vehicle crossovers should be limited to service roads only. It was suggested that the DDOs may be strengthened if they were to require a Movement and Place Assessment for the whole transport network.</p> <p>Suggested drafting changes:</p> <ul style="list-style-type: none"> • Amend the BUDF to replace the title “Movement and Connectivity Framework” to be “Movement and Place Framework” so that it references the Movement and Place in Victoria Framework (Department of Transport, February 2019). • Amend the wording of the BUDF to ensure that raised vehicle thresholds at pedestrian crossings are limited to service roads only. 	Positive	<p>Amending the title Movement and Connectivity in the BUDF and amending the wording to ensure that raised vehicle crossings are limited to the service roads only are not supported. The BUDF was adopted by Council on 22 December 2022 and making changes to the BUDF is not within the scope of this amendment to implement it.</p> <p>It is noted that improvements to the state transport system is dependent on the State’s intervention and funding.</p> <p>Requiring a Movement and Place Assessment for the whole transport network via the DDO6&19 is not supported. Whilst a Movement and Place Assessment may be beneficial for the future planning of the Boulevards, the Design and Development Overlay is the wrong mechanism to require it.</p> <p>The change of wording to the DDO6 to include the phrase “and where possible avoid vehicle access to Transport Zone 2” is supported. There is no Transport Zone 3 in Monash, therefore the reference to Transport Zone 3 is unnecessary.</p> <p>Requiring a traffic impact assessment for each permit application will ensure that due consideration is given to the effects to traffic by the proposed development and is supported. However, it will be more appropriate to require it at 5.0 Application requirements in the DDO6 under the heading <i>Applications for non-accommodation buildings of three storeys or less, and accommodation buildings of four storeys or more</i> and 5.0 at DDO19.</p> <p>Specifying to avoid vehicle access to Transport Zone 2 and 3 in the DDO19 is unnecessary and not supported. The site has access to side streets and a service lane and does not have direct access to Dandenong Road.</p>

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
	<ul style="list-style-type: none"> • Amend the DDO6 to avoid vehicle access to Transport Zone 2 and 3 where possible. • Amend the DDO6 to require the permit applicant to satisfy a Transport Impact Assessment including Movement and Place principles. • Amend the DDO19 to require the provision of a Transport Impact Assessment including Movement and Place Principles. • Amend the DDO19 to specify that where possible avoid vehicle access to Transport Zone 2 and 3. 		<p>Changes are recommended to Amendment C172 in response to this submission to:</p> <ul style="list-style-type: none"> • Include the phrase “and where possible avoid vehicle access to Transport Zone 2” in DDO6 • Require a Traffic Impact Assessment for applications for non-accommodation buildings of three storeys or less and accommodation buildings of four storeys or more within the DDO6 and DDO19.
SUB26, Human Habitats on behalf of company in DR4	<p>The submission objects to the amendment and specifically relates to the property at 1881-1889 Dandenong Road and 135 Clayton Road, Oakleigh East. The submission is generally supportive of the proposed implementation, however, raise concern with the proposed built form requirements for this site in particular. The issues raised in the submission are listed below with their answers in the matching column across.</p>	Mixed	

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	<ul style="list-style-type: none"> It is not clear how the scale of development will be assessed and how the strategies will be enforced. 		<p>The proposed strategies at Clause 15.01-5L are the high level, intending to set the desired future outcome for the Boulevards. The strategies will be 'enforced' through the tools of the planning scheme: the GRZ4, RGZ7, MUZ3, DDO6 and DDO19</p>
	<ul style="list-style-type: none"> The transition between adjoining land in a different zone which has lower maximum height levels and the proposed maximum height. 		<p>The transition between maximum height levels of different zones will be managed by the rear setback requirements of the DDO6.</p> <p>DDO6 and DDO19 contain rear and side setback requirements that are intended to protect the amenity of neighbouring residential properties. When the development meets the setback requirements, it is considered that a suitable transition from lower scale areas to higher scale has been achieved.</p> <p>I</p> <p>There are adequate decision guidelines proposed to allow for a full consideration of the transitions from low-scale to taller building heights.</p>
	<ul style="list-style-type: none"> Is the maximum building height of the RGZ7 mandatory or discretionary? 		<p>The new height limits proposed for the areas to be rezoned RGZ7 are discretionary. The height limits are proposed by the DDO6 and DDO19 rather than the RGZ7 due to a condition of authorisation (removing the ability for us to make them mandatory). There remains a question about the interaction between the RGZ and the DDO6, in that the RGZ sets a maximum discretionary height of 13.5m (4 storeys) in its parent provision, and the DDO6 allows for a potentially taller height of 21m (6 storeys). There is a decision guideline that states that the DDO6 prevails across any inconsistency with the zone. However, it would be clearer if the maximum height limits were specified as mandatory controls in the RGZ. Council should seek advice from the planning panel for a resolution.</p>
	<ul style="list-style-type: none"> Disputes the minimum street setback of 7.6 metres to Dandenong Road. It will significantly limit development opportunities and contradicts the intention of the zone. The 7.6 metre 		<p>The 7.6 metre front street setback is intended to enhance the 'Boulevard Character'. 7.6 metre front setback is required at a minimum to ensure planted trees will survive and have sufficient room for large canopy coverage and has been a consistent setback applied in Monash for many years and is the front setback in the current zone (GRZ2). Basement carpark entrances are discouraged from encroaching into the boulevard setback to</p>

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	<p>setback should be reduced. Also raises concerns that the 7.6 metre setback will not allow basement carparks to encroach within 7.6 metres of the sites frontage.</p>		<p>ensure that there is a consistent landscaped setback, sufficient space for deep soil zones for canopy trees, and minimal hard surfacing. A permit can be granted to allow basements to encroach into this setback, if designed well and landscaping is otherwise maximised.</p>
	<ul style="list-style-type: none"> Reduce the secondary front setback [side setback] requirement of 3 metres where active interfaces can be achieved by other methods. 		<p>Not supported. The objective of the RGZ7 is to provide consistent side and rear setbacks that allow generous space for landscaping and visual consistency. Reducing the secondary setback requirement will not allow consistency between setbacks, nor provide the adequate space needed for landscaping, including canopy trees. It will compromise the objective of the RGZ7.</p>
	<ul style="list-style-type: none"> It is excessive to provide for one street tree every 5 metres. 		<p>Providing at least one canopy tree every five metres of site frontage will achieve the objective of the RGZ7 to provide a consistent strongly landscaped setback along each boulevard, enhancing the garden city character of Monash. It should be noted that not all trees are required to be in the front setback, some can be located elsewhere on site.</p>
	<ul style="list-style-type: none"> The requirement for 30% permeability is excessive. Suggested that the permeability requirement is no different from Clause 55 at 20%. 		<p>The intention requiring 30% permeability is to encourage visual separation and landscaping between buildings. It will assist with establishing the 'Boulevard Setback' a consistent space for landscaping across the Boulevards. If more space for development is required, then lot consolidation should be considered.</p>
	<ul style="list-style-type: none"> Dispute the 75 square metres required for private open space. This is also considered excessive. 		<p>Requiring 75 square metres of private open space will only apply to Standard A17, which applies to dwellings on lots of less than 300 square metres. There is no proposed variation to Standard B28.</p>

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	<ul style="list-style-type: none"> Does the DDO6 allow for additional height beyond the maximum height limits? 		<p>The wording of the DDO6 is “building heights should not exceed the heights identified in Table 1...”. The wording provides discretion for taller building heights to be assessed by Council’s Statutory Planning Department. It is only possible to exceed the height limit on land in the RGZ7.</p>
	<ul style="list-style-type: none"> Request a discretionary front and side setback requirement to be based on the existing streetscape character of the area. 		<p>See above response regarding discretionary front and side setbacks.</p>
	<ul style="list-style-type: none"> Requiring 60% of the front setback to be landscaped diminishes development opportunities for the site. It will limit creativity and unique interface design. 		<p>Noted. The 60% landscaping requirement within the boulevard setback will support the design objectives of the DDO6 “to provide a consistent, strongly landscape setback along each boulevard, enhancing the garden city character of Monash” and the intention of the BUDF.</p>
	<ul style="list-style-type: none"> The proposed 10.6 metre upper level setbacks will impact residential development on narrow allotments. 		<p>The total 10.6 metre setback from the boulevard boundary will ensure that there is a consistent human scale of buildings fronting the boulevards.</p>
SUB27, Resident in DR4	<p>The Submitter does not support the increase in height limit to 4 and 6 storeys, nor the existing 3 storey height limit. Concern over increase to traffic congestion and damage to parked vehicles on narrow streets. Overshadowing, lack of airflow and pressure on existing infrastructure are also identified. Includes complaints about existing development of up to three stories including impacts to water pressure, traffic congestion,</p>	Negative	<p>See response to submission 9 regarding traffic concerns. See response to submission 10 regarding overshadowing concerns.</p> <p>This is an established suburban area that is connected to reliable potable water supply, managed by Yarra Valley Water (northern side of Dandenong Road) and South East Water (southern side of Dandenong Road). These agencies adjust their infrastructure based on projected demand, and have been notified about Amendment C172.</p> <p>Visual bulk may seem depressive and intrusive to some residents however, if designed using good design principles this feeling can be eliminated or</p>

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
	<p>overshadowing and visual bulk affecting mental health.</p> <p>The submitter requests the maximum building height to be reduced to no more than 2 storeys, specifically at 1566 and 1568 Dandenong Road.</p>		<p>reduced. The design requirements of the DDO6 and DDO19 work to combat this by ensuring wide upper-level setbacks.</p> <p>Reducing the maximum height limit to be no more than 2 storeys is out of scope of the amendment, and inconsistent with the adopted Monash Housing Strategy and state, regional and local planning policies. The NRZ has been appropriately applied to areas of special significance, where heights should be limited to 2 storeys.</p> <p>No changes are recommended to the amendment in response to this submission.</p>
SUB28, Resident in DR6	<p>The submitter strongly objects to the amendment on grounds of loss of neighbourhood character, overshadowing, devaluing land, increase to traffic congestion and decrease to quality of life. Suggests the target of the rezoning is 2263 Dandenong Road.</p>	Negative	<p>4 storeys is appropriate for land within proximity of the Monash Technology Precinct. 27 metres is appropriate for land at a major intersection with commercial and industrial land in close proximity. The land to be rezoned RGZ7 with a maximum height limit of 6 storeys will provide the appropriate transition from 8 storeys (to the east) to 4 storeys.</p> <p>See response to submission 10 regarding impacts to neighbourhood character, overshadowing and loss of privacy.</p> <p>See response to submission 9 regarding increase in traffic congestion.</p> <p>No single property is the specific target of the amendment, although there are site specific provisions for the key strategic redevelopment site at 2277 Dandenong Road.</p> <p>Assessments based on increases or decreases to property value is out of scope of the planning assessment.</p> <p>No changes are recommended to the amendment in response to this submission.</p>

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
SUB29, Resident adjoining SR5	<p>Submitter objects to the amendment and requests the plans be adjusted to sensitively respect the use of properties in SR5.</p> <p>Concerns over neighbourhood character, loss of views, visual bulk, overshadowing and overlooking. Additionally, the Submitter identifies light and noise pollution as concerns, especially to wildlife. The infrastructure will be stressed and taller buildings will create wind tunnels. Roads and footpaths will deteriorate due to increased congestion. That it will contribute to the affects of climate change.</p>	Negative	<p>See response to submission 10 regarding overshadowing, overlooking and neighbourhood character.</p> <p>The amendment allows for an increased density of residential development in existing residential areas fronting main six land roads. Given the existing built environment, it is unlikely that there will be wildlife in these areas that would be further adversely affected.</p> <p>Pressure on infrastructure can be mitigated through engineering and design solutions. It is the responsibility of the road authority, either Council or Vic Roads to repair and replace roads and footpaths when appropriate.</p> <p>Providing homes for future residents of Melbourne within areas that have access to existing services will do more to mitigate climate change than developing within growth areas which will destroy more intact natural environments and increase pollution through more construction and long-term effects of intensified car use for travel to and from these areas.</p> <p>No changes are recommended to the amendment in response to this submission</p>
SUB 30, Resident adjoining SR4	<p>Submission is in relation to the effects of rezoning 523-535 Springvale Road, Glen Waverly to have a maximum height limit of 21 metres or 6 storeys on 86 Winmallee Drive, Glen Waverly. Concern over increased traffic to the intersection of Winmallee Drive and Springvale Road and lack of on-street carparking to Winmallee Drive and Kerford Road.</p>	Negative	<p>See response to Submission 10 to address concerns over increased traffic and on-street car parking.</p> <p>No changes are recommended to the amendment in response to this submission</p>

Submission no. and details	Key issues raised (or verbatim comments)	Sentiment	Officer comments and recommendations
SUB31 – APA Group	No objections to the amendment. Supports guideline G26 in the BUDF.	Support	No changes are sought and no changes are recommended to the amendment in response to this submission
SUB32, Melbourne Water	<p>The submission does not object to the amendment but contains general advice about the 1% AEP flood event extent.</p> <p>Recommends that intensification in several areas should be discouraged as they are subject to flooding above 0.3 metres. These areas include:</p> <ul style="list-style-type: none"> • North of Mackie Road • East of Kalimna Avenue <p>However, the submission does not seek any changes.</p>	Support	<p>North of Mackie Road and east of Kalimna Avenue will be rezoned to GR24 with a maximum height limit of 4 storeys or 13.5 metres. These properties are within the SBO, therefore any application must be referred to Melbourne Water as the determining referral authority. Increasing the height limit by one storey is unlikely to contribute to flooding impacts.</p> <p>No changes are recommended to the amendment in response to this submission</p>

MONASH PLANNING SCHEME

Proposed C172mona

SCHEDULE 6 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO6**.

THE MONASH BOULEVARDS

1.0

Proposed C172mona

Design objectives

To provide a consistent, strongly landscaped setback along each boulevard, enhancing the garden city character of Monash.

To provide high quality and environmentally sustainable mid-rise development along each boulevard, with taller built form located at key gateways.

To ensure the height and built form of new buildings provide an acceptable interface, amenity outcomes and transition to adjoining lower scale residential areas in the General Residential Zone and Neighbourhood Residential Zone.

To encourage lot consolidation in order to achieve the maximum building heights and to provide for sufficient building setbacks to deliver high levels of internal and external amenity.

To minimise the need for fencing along street interfaces through appropriate landscaping and building design.

2.0

Proposed C172mona

Buildings and works

A permit is not required to construct a building or construct or carry out works for a development up to and including 3 storeys if the following conditions are met:

- No part of a building, including a basement, is constructed within 7.6m from the boundary with Dandenong Road, Springvale Road or a service road accessed from Dandenong Road or Springvale Road;
- A minimum of 60% of the above setback is landscaped and free of impermeable surfaces; and
- The building is used for the purposes of accommodation.

A permit is required to construct a fence with any street interface.

The following buildings and works requirements apply to an application to construct a building, construct or carry out works:

Definitions

'Boulevard' means Dandenong Road, Springvale Road or a service road accessed from Dandenong Road or Springvale Road.

'Boulevard boundary' means a lot boundary shared with a boulevard.

'Boulevard setback' means the space between the boulevard boundary and any buildings.

Building height

- Building heights should not exceed the heights identified in Table 1 relative to the site width requirements.
- Building heights above 11 metres (up to three storeys) should not exceed 13.5 metres (up to four storeys) beyond 50 metres from the *boulevard boundary*, regardless of the applicable maximum building height in Table 1.

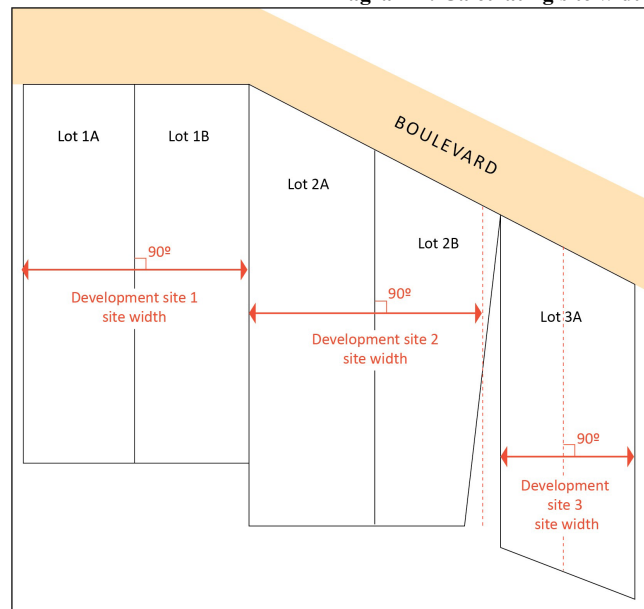
MONASH PLANNING SCHEME

Table 1: Building height and lot width

Height area	Site width*	Preferred building height
Area A (GRZ4)	Less than 24 metres in width	11 metres up to 3 storeys
	24 metres or greater in width	13.5 metres up to 4 storeys
Area B (RGZ7)	Less than 30 metres in width	13.5 metres up to 4 storeys
	30 metres or greater in width	21 metres up to 6 storeys

*Note - As illustrated in Diagram 1, 'site width' is calculated by combining the widths of all lots making up the development site as measured perpendicular to the side boundary. Where side boundaries are not parallel, the average site width is used.

Diagram 1: Calculating site widths



Boulevard setback

- . All parts of a building should be set back a minimum of 7.6 metres from the *boulevard boundary*. This includes any part of a basement, excluding an access ramp. (Refer to Diagrams 2 & 3 below)
- . Upper levels above 13.5 metres (four storeys) should be set back an additional 3.0 metres from the *boulevard boundary* (10.6 metres in total). (Refer to Diagram 3 below)
- . A minimum of 60% of the boulevard setback should contain landscaping and pervious surfaces.
- . Canopy trees should be planted within the *boulevard setback* to achieve a mature height greater than the predominant height of the building visible from the street and with sufficient canopy spread to shade to at least 80% of the *boulevard setback*. For the purposes of this requirement, the predominant height will be 13.5m (or the height of the development, whichever is lesser) in Area A or if levels above 13.5m are setback an additional 3.0m from the *boulevard* in Area B.

MONASH PLANNING SCHEME

- Secluded private open space areas at ground level should not be provided within the *boulevard setback*. Instead, these areas should be provided within the other setbacks.

Other setbacks

Corner and dual frontage sites

- A building should be set back at least 3.0 metres from the intersecting side street. (Refer to Diagrams 5 & 6)
- Upper levels above 13.5 metres (four storeys) should be set back an additional 3.0 metres from the intersecting side street (6.0 metres in total). (Refer to Diagram 6)
- Where a site has more than one frontage, the setback from the secondary (non-boulevard) frontage should respect the setbacks of the surrounding built form.

Side setbacks

- Buildings should be set back 1.0m plus 0.3m for every metre of height over 3.6m up to 10.1 metres, plus 1 metre for every metre of height over 10.1m, up to 16.9m). (Refer to Diagrams 5 & 6)
- On sites within Area B that abut a site in Area A or a residential zone that allows for heights up to 4 storeys, ensure that buildings exceeding 16.9 metres (5 storeys) in height provide a transition in height along that interface.

Rear setbacks

- Buildings should be set back 4.0m from the rear boundary for development up to 10.1m (3 storeys), plus 1.0m additional setback for every metre of height over 10.1m up to 16.9m (5 storeys). Rear setback to be taken from the rear boundary, and not the centre of a laneway (if applicable). (Refer to Diagrams 2 & 3)
- Where a habitable room window, balcony, terrace, deck or patio faces a common side or rear boundary, provide a setback of 4.5m to the common boundary.
- Where a site directly abuts a property within the Neighbourhood Residential Zone, provide 4.0m rear setback for development up to 6.8m (2 storeys), plus 1.0m additional setback for every metre of height over 6.8m up to 13.5m (4 storeys). (Refer to Diagram 4)

Diagram 2: Boulevard and rear setbacks in Area A

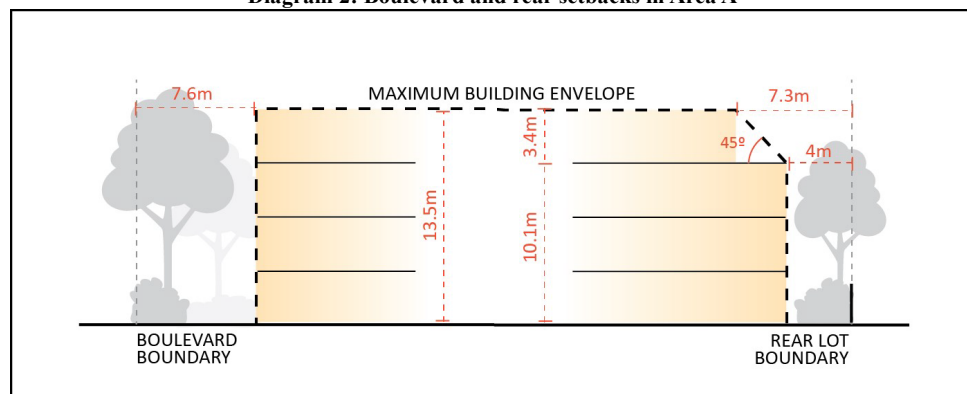


Diagram 3: Boulevard and rear setbacks in Area B

MONASH PLANNING SCHEME

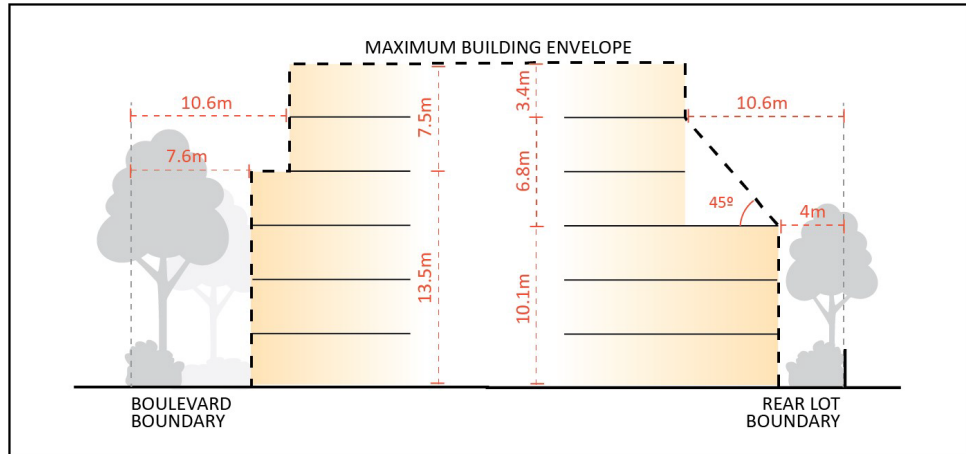


Diagram 4: Rear setbacks with NRZ

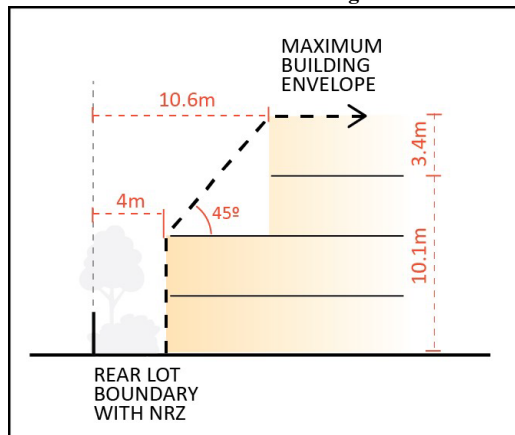


Diagram 5: Side boundary and side street setbacks in Area A

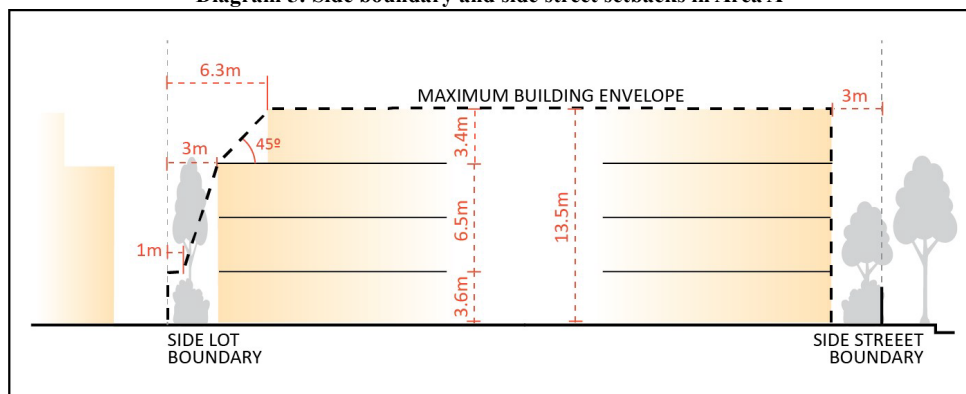
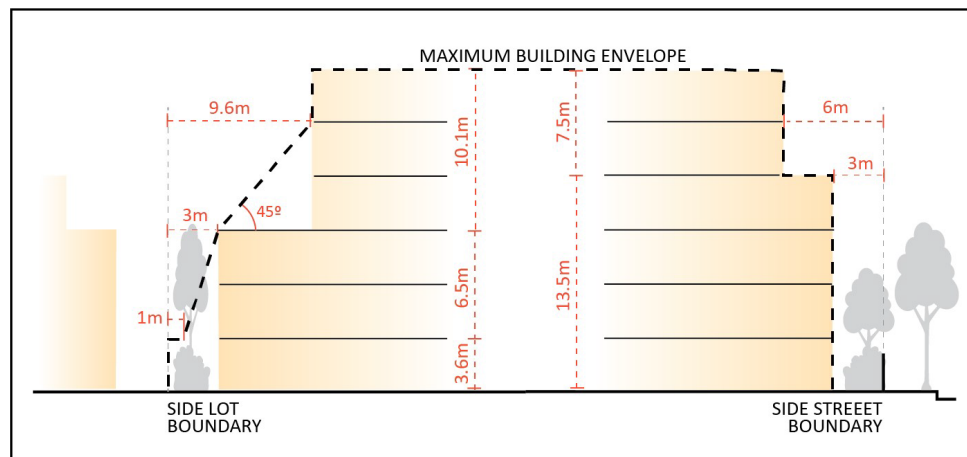


Diagram 6: Side boundary and side street setbacks in Area B

MONASH PLANNING SCHEME



Building form, orientation and design

- For sites adjoining heritage places, provide a transition in scale and form to respect and integrate with the heritage character and significance of the adjoining heritage places.
- Building façades should be designed to:
 - Be sufficiently articulated to avoid visual bulk through the considered design of openings, balconies, varied materials, recessed and projected elements, opportunities for vegetation, and revealing structural elements such as columns and beams.
 - Avoid large areas of rendered surfaces.
 - Avoid a ‘wedding cake’ built form outcome through upper levels on buildings of five storeys or more set back in a minimum of two steps.
 - ~~Allow for the interpretation or reading of each floor level of the building.~~
 - Be simple and articulated, and not rely on excessive use of materials to achieve visual interest.
- Roof forms should be designed to:
 - Be articulated or divided into distinct sections on larger buildings in order to minimise visual bulk and respond to the roof proportions of existing buildings.
 - Consider site orientation so that element such as eaves and external shades can respond to solar access and shading needs.
 - Consider the architectural detail of eaves as part of the design.
- Services and equipment above the roof (such as plant, lift cores, heating and cooling) should be well integrated, not face the street and contained within the roof form or screened behind a parapet so that they are not visible.
- Site services, such as meter boxes, fire fighting equipment and mail boxes, should be incorporated into the design of the building.
- Buildings to include provision for rainwater capture, storage and re-use, and future readiness for connection to recycled water.
- Provide a sense of address and human scale interaction with the street through:

MONASH PLANNING SCHEME

- Ground level occupation and the presence of habitable rooms and balconies at all levels.
- Placing inactive uses, such as laundries, garages and bathrooms, away from street-facing interfaces (where practicable).
- Utilise modulation to distinguish between individual dwellings.
- Activated and landscaped interfaces along all street frontages.
- Separate entries to individual dwellings at ground level.
- Clearly defined building entries directly front the street.
- Where private open space is located at ground floor and visible from a street, innovative techniques should be used to define and give privacy to areas in place of solid fencing. This may include the use of raised garden beds or decorative screening and permeable fencing.
- Buildings should be sited to:
 - Facilitate natural light and ventilation to windows and openings, including cross-ventilation breezes.
 - Mitigate noise and air pollution from the main road environment and, in some cases, nearby industrial uses, to noise sensitive rooms and balconies.
 - Allow for adequate light and sun penetration to existing and future development on adjoining properties.
 - Designed and orientated to maximise opportunities for solar access to living areas and private open space.
 - Locate driveways on the south side of lots with an east-west orientation to maximise solar access to habitable spaces and minimise overshadowing of neighbouring properties.
 - Respond to existing conditions including adjoining uses, topography, vegetation and views.

Car parking and building access, pedestrians and cycling .

Vehicle crossovers and car parking access should be designed to:

- Provide a single vehicle crossover per site.
- Be provided from the intersecting side street rather than the Boulevard interface or service lanes, if the site is on a corner.
- Be positioned away from existing street trees to avoid root damage and/or removal.
- Avoid vehicle access to Transport Zone 2 where possible.
- Pedestrian entries, routes between public areas and site facilities, and access should be designed to:
 - Be visible, and accessible to all people, including those with limited mobility, and those with bikes, prams, wheelchairs and mobility scooter.
 - Have integrated pedestrian access ramps and stairs, without compromising the extent of landscaping within street setbacks.
 - Be easy to locate and orientated to address the *boulevard boundary*.
 - Have planting and landscape treatments.

MONASH PLANNING SCHEME

- Facilitate ease of evacuation to side and rear streets in the event of high pressure gas pipeline failure along Dandenong Road.
- . The location of bicycle parking should be easily accessible from the street and at ground level and should be in an area of the site subject to passive or active surveillance.
- . Ensure new apartment developments have capacity and readiness for EV charging.

Landscaping and fencing

- . Landscaping should be designed to:
 - Prioritise the retention of significant and large canopy trees.
 - Maximise deep soil planting zones within boulevard interface and rear setbacks.
 - Have canopy vegetation of a suitable size and height which emerges above the predominant form of the proposed development visible from the street.
 - Be a minimum of 60 per cent of the total area of the boulevard interface, incorporating grassed and planted areas.
 - Have vertical greening within side (and side street) setbacks to create the effect of the buildings sitting in a landscaped setting. This will preferably include trees with a narrow canopy or shrubs or climbers on fences / walls are to reach a minimum of 1.8 metres high where trees are not feasible.
 - Utilise green roofs, walls and balconies to provide additional landscaping and soften the visual impact of buildings.
 - Incorporate fencing solutions.
- . Water run-off from impervious surfaces should be collected, cleaned and re-used through passive irrigation where practical.
- . Where paved surfaces are required, position trees and built form to ensure these are at least partially shaded during summer.
- . Encourage the use of permeable paving surfaces, where feasible, to assist with overall soil moisture content.
- . Fences along street interfaces, particularly the boulevard interface, should be avoided to create open and welcoming entrances. However, if fences are required they should be designed to:
 - Not exceed a maximum height of 1.2 metres.
 - Be designed to incorporate landscaping and permeability to contribute greenery and provide a level of passive surveillance to the street.
 - Avoid high fencing along footpaths.

3.0

Proposed C172mona

Subdivision

None specified.

4.0

Proposed C172mona

Signs

None specified.

MONASH PLANNING SCHEME

5.0

Proposed C172mona

Application requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

Applications for a fence

- Plans showing proposed fencing, including height, materials and transparency.

Applications for a buildings of three storeys or less

- Plans showing the extent of walls and other architectural features extending into the 7.6 metre boulevard setback.
- Plans showing existing vegetation and any trees proposed to be removed.
- Plans showing proposed landscaping works, permeable/impervious surfaces, and planting including tree species and mature height.
- A schedule of materials and finishes to be used in the development.
- The location and number of vehicle crossovers.
- A statement about how the development meets the requirements of this schedule.

Applications for non-accommodation buildings of three storeys or less, and accommodation buildings of four storeys or more

- A landscape plan prepared by a landscape architect or a suitably qualified landscape designer, drawn to scale and dimensioned which:
 - Identifies, retains and protects significant vegetation on the site and significant vegetation on adjoining properties in proximity to the development which contributes to the character of the area, including the identification of tree protection zones.
 - Proposes new canopy trees and other vegetation that will enhance the landscape character of the neighbourhood particularly within the front, side and rear setbacks, along driveways and walkways, and within private open space areas.
 - Provides a schedule of all proposed trees, shrubs and ground covers including the size of all plants (at planting and at maturity), their location, botanical names and the location of all areas to be covered by grass, lawn, mulch or other surface material.
 - Provides the location and details of all fencing and external lighting.
 - Identifies the extent of any cut and fill, embankments or retaining walls associated with the landscape treatment of the site.
 - Details of all proposed surface materials including pathways, patios or decked areas and measures to reduce stormwater runoff such as porous paving, swales and infiltration, ponding areas and grey water reuse.
- A schedule of materials and finishes to be used in the development.
- A plan identifying service areas, such as waste and recycling areas, utilities and services.
- Details of screening to waste and recycling areas if provided external to the building.
- [A Transport Impact Assessment.](#)
 - A statement about how the development meets the requirements of this schedule.

MONASH PLANNING SCHEME

6.0 Decision guidelines

~~Proposed C172mona~~

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the development meets the building heights, building setbacks, building form and design, access, landscaping and fencing requirements of this schedule.
- Whether there is an appropriate balance between greening of the site and providing for reasonable levels of accommodation and access.
- The practicality of providing ground level setbacks on all interfaces, particularly on smaller and irregularly shaped sites.
- The design and placement of vehicle access to avoid the removal or harm to existing street trees.
- Whether there is an appropriate balance between simple architectural detail and articulation to avoid visual bulk.
- ~~Where a permit is required for the development under the General Residential Zone or the Residential Growth Zone, any inconsistency between the requirements of the schedule to the RGZ and this schedule. In the event of any inconsistency, this schedule prevails.~~
- Where there are any inconsistencies between the objectives, permit requirements, building heights, design considerations and decision guidelines of the General Residential Zone, Residential Growth Zone and this schedule, this schedule - Schedule 6 to the Design and Development Overlay prevails.

MONASH PLANNING SCHEME

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Proposed C172mona

SCHEDULE 19 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY

Shown on the planning scheme map as **DDO19**.

THE MONASH BOULEVARDS KEY DEVELOPMENT SITE - 2277 DANDENONG ROAD, MULGRAVE

1.0

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Proposed C172mona

Design objectives

To provide a consistent, strongly landscaped setback along Dandenong Road, enhancing the garden city character of Monash.

To provide high quality, visually interesting, environmentally sustainable, contemporary building(s) at this key gateway site near the intersection of Dandenong Road and Springvale Road.

To ensure the height and built form of new buildings provide an acceptable interface, amenity outcomes and transition to adjoining lower scale residential areas in the General Residential Zone.

To minimise the need for fencing along street interfaces through appropriate landscaping and building design.

2.0

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Proposed C172mona

Buildings and works

A permit is required to construct a fence with a street interface.

Note: a permit is required to construct a building, construct or carry out works.

The following buildings and works requirements apply to an application to construct a building, construct or carry out works:

Definitions

'**Boulevard**' means Dandenong Road, Springvale Road or a service road accessed from Dandenong Road or Springvale Road.

'**Boulevard boundary**' means a lot boundary shared with a boulevard.

'**Boulevard setback**' means the space between the boulevard boundary and any buildings.

Building height

Building heights should not exceed 27 metres (up to 8 storeys).

Boulevard setback

. All parts of a building should be set back a minimum of 7.6 metres from the *boulevard boundary*. This includes any part of a basement, excluding an access ramp.

. Upper levels above 13.5m (4 storeys) should be set back an additional 3.0 metres from the *boulevard boundary* (10.6 metres in total). (Refer to Diagram 1)

. A minimum of 60% of the *boulevard setback* should contain landscaping and pervious surfaces.

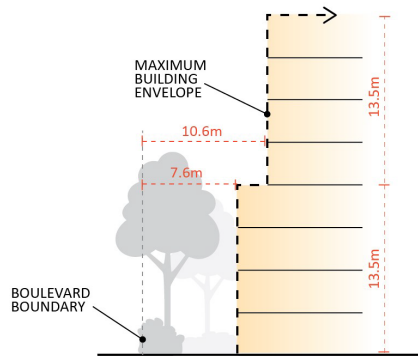
. Canopy trees should be planted within the boulevard setback to achieve a mature height greater than the height of the building and with sufficient canopy spread to shade to at least 80% of the boulevard setback.

. Canopy trees should be planted within the *boulevard setback* to achieve a mature height greater than the predominant height of the building visible from the street and with sufficient canopy spread to shade to at least 80% of the *boulevard setback*. For the purposes of this requirement, the predominant height will be 13.5m (or the height of the development, whichever is lesser).

MONASH PLANNING SCHEME

- Secluded private open space areas at ground level should not be provided within the *boulevard setback*. Instead, these areas should be provided within the side and rear setbacks

Diagram 1: Boulevard setback



Other setbacks

A building should be set back at least 3.0m from Harcourt Avenue.

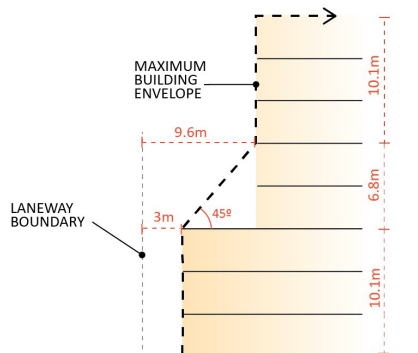
- Upper levels above four storeys (13.5m) should be set back an additional 3.0m from Harcourt Avenue (6.0m in total).

A building should be set back at least 4.0m from Wilma Avenue.

- Upper levels on the Wilma Avenue interface above 6.8m (2 storeys) should be set back 1.0m for every metre of height over 6.8m, up to 16.9m, and avoiding a 'wedding cake' built form outcome.
- A building should be set back at least 3.0m from the laneway at the rear of the Gateway Shopping Centre.
- Upper levels along the laneway interface should be set back 0.3m for every metre of height over 3.6m up to 10.1m (3 storeys), plus 1 metre of height over 10.1m, up to 16.9m. (Refer to Diagram 2)

Diagram 2: Laneway interface setback

MONASH PLANNING SCHEME



Building form, orientation and design

- Provide building modulation to Dandenong Road, Springvale Road and Harcourt Avenue that reflects the grain of the adjoining land subdivision to the north west and north east.

Avoid monolithic forms.

- Vary ground level setbacks to reduce visual bulk and create additional opportunities for landscaping.
- Building façades should be designed to:
 - Be sufficiently articulated to avoid visual bulk through the considered design of openings, balconies, varied materials, recessed and projected elements, opportunities for vegetation, and revealing structural elements such as columns and beams.
 - Avoid large areas of rendered surfaces.
 - Avoid a ‘wedding cake’ built form outcome through upper levels on buildings of five storeys or more set back in a minimum of two steps.
 - Allow for the interpretation or reading of each floor level of the building.
 - Be simple and articulated, and not rely on excessive use of materials to achieve visual interest.
- Roof forms should be designed to:
 - Be articulated or divided into distinct sections on larger buildings in order to minimise visual bulk and respond to the roof proportions of existing buildings.
 - Consider site orientation so that element such as eaves and external shades can respond to solar access and shading needs.
 - Consider the architectural detail of eaves as part of the design.
- Services and equipment above the roof (such as plant, lift cores, heating and cooling) should be well integrated, not face the street and contained within the roof form or screened behind a parapet so that they are not visible.

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- Site services, such as meter boxes, fire fighting equipment and mail boxes, should be incorporated into the design of the building.
- Buildings to include provision for rainwater capture, storage and re-use, and future readiness for connection to recycled water.
- Provide a sense of address and human scale interaction with the street through:
 - Ground level occupation and the presence of habitable rooms and balconies at all levels.
 - Placing inactive uses, such as laundries, garages and bathrooms, away from street-facing interfaces (where practicable).
 - Utilise modulation to distinguish between individual dwellings.
 - Activated and landscaped interfaces along all street frontages.
 - Separate entries to individual dwellings at ground level.
 - Clearly defined building entries directly front the street.
- Where private open space is located at ground floor and visible from the street, innovative techniques should be used to define and give privacy to area in place of solid fencing. This may include the use of raised garden beds or decorative screening and permeable fencing.
- Buildings should be sited to:
 - Facilitate natural light and ventilation to windows and openings, including cross-ventilation breezes.

MONASH PLANNING SCHEME

- Mitigate noise and air pollution from the main road environment and nearby industrial uses, to noise sensitive rooms and balconies.
- Allow for adequate light and sun penetration to existing and future development on adjoining properties.
- Designed and orientated to maximise opportunities for solar access to living areas and private open space.
- Locate driveways on the south side of lots with an east-west orientation to maximise solar access to habitable spaces and minimise overshadowing of neighbouring properties.
- Respond to existing conditions including adjoining uses, topography, vegetation and views.

Car parking and building access, pedestrians and cycling

- Provide pedestrian access between the site and shops.
- Provide ground level setback of 3 metres to the existing laneway along the south eastern boundary of to support laneway widening and provide buffer to adjoining retail uses.
- Focus vehicle access to car parking from Harcourt Avenue and Wilma Avenue (including via the laneway from Wilma Avenue).
- Vehicle crossovers and car parking access should be designed to:
 - Provide a single vehicle crossover per site.
 - Be provided from the intersecting side street rather than the Boulevard interface or service lanes, if the site is on a corner.
 - Be positioned away from existing street trees to avoid root damage and/or removal.
 - Pedestrian entries, routes between public areas and site facilities, and access should be designed to:
 - Be visible, and accessible to all people, including those with limited mobility, and those with bikes, prams, wheelchairs and mobility scooter.
 - Have integrated pedestrian access ramps and stairs, without compromising the extent of landscaping within street setbacks.
 - Be easy to locate and orientated to address the *boulevard boundary* .
 - Have planting and landscape treatments.
 - Facilitate ease of evacuation to side and rear streets in the event of high pressure gas pipeline failure along Dandenong Road.
 - The location of bicycle parking should be easily accessible from the street and at ground level and should be in an area of the site subject to passive or active surveillance.
 - Ensure new apartment developments have capacity and readiness for EV charging.

Landscaping and fencing

- Landscaping should be designed to:
 - Prioritise the retention of significant and large canopy trees.
 - Maximise deep soil planting zones within boulevard interface and rear setbacks.

MONASH PLANNING SCHEME

- Have canopy vegetation of a suitable size and height which emerges above the predominant form of the proposed development visible from the street.
 - Be a minimum of 60 per cent of the total area of the boulevard interface, incorporating grassed and planted areas.

Have vertical greening within side street setbacks to create the effect of the buildings sitting in a landscaped setting. This will preferably include trees with a narrow canopy or shrubs or climbers on fences / walls are to reach a minimum of 1.8 metres high where trees are not feasible.
 - Utilise green roofs, walls and balconies to provide additional landscaping and soften the visual impact of buildings.
 - Incorporate fencing solutions.
- Water run-off from impervious surfaces should be collected, cleaned and re-used through passive irrigation where practical.
 - Where paved surfaces are required, position trees and built form to ensure these are at least partially shaded during summer.
 - Encourage the use of permeable paving surfaces, where feasible, to assist with overall soil moisture content.
 - Fences along street interfaces, particularly the boulevard interface, should be avoided to create open and welcoming entrances. However, if fences are required they should be designed to:
 - Not exceed a maximum height of 1.2 metres.
 - Be designed to incorporate landscaping and permeability to contribute greenery and provide a level of passive surveillance to the street.
 - Avoid high fencing along footpaths.

3.0

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Proposed C172mona

Subdivision

A permit is not required to subdivide land.

4.0

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Signs

None specified.

5.0

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Proposed C172mona

Application requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- A landscape plan prepared by a landscape architect or a suitably qualified landscape designer, drawn to scale and dimensioned which:
 - Identifies, retains and protects significant vegetation on the site and significant vegetation on adjoining properties in proximity to the development which contributes to the character of the area, including the identification of tree protection zones.
 - Proposes new canopy trees and other vegetation that will enhance the landscape character of the neighbourhood particularly within the front, side and rear setbacks, along driveways and walkways, and within private open space areas.

MONASH PLANNING SCHEME

- Provides a schedule of all proposed trees, shrubs and ground covers including the size of all plants (at planting and at maturity), their location, botanical names and the location of all areas to be covered by grass, lawn, mulch or other surface material.
 - Provides the location and details of all fencing and external lighting.
 - Identifies the extent of any cut and fill, embankments or retaining walls associated with the landscape treatment of the site.
 - Details of all proposed surface materials including pathways, patios or decked areas and measures to reduce stormwater runoff such as porous paving, swales and infiltration, ponding areas and grey water reuse.
- A schedule of materials and finishes to be used in the development.
- A plan identifying service areas, such as waste and recycling areas, utilities and services.
- Details of screening to waste and recycling areas if provided external to the building.
- A statement about how the development meets the requirements of this schedule.
- [A Transport Impact Assessment.](#)

6.0 Decision guidelines

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The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- Whether the development meets the building heights, building setbacks, building form and design, access, landscaping and fencing requirements of this schedule.
 - Whether the placement of any structures, including porches, balconies and architectural features within the required front, side and rear setbacks are reasonable due to their lightweight design.
 - Whether there is an appropriate balance between heavy greening to the site and providing for reasonable levels of accommodation and access.
- The design and placement of vehicle access to avoid the removal or harm to existing street trees.
- Whether there is an appropriate balance between simple architectural detail and articulation to avoid visual bulk.